



UNIFYING THROUGH CULTURE AND COMMERCE

THE DALLAS ARTS DISTRICT CONNECT MASTER PLAN

DRAFT

December 2020

**DALLAS
ARTS
DISTRICT**

The **Dallas Arts District** is a nonprofit 501(c)3 that serves as the primary steward and representative for the District and is funded by grants, voluntary membership dues, sponsorships, and donations. In 2009, the Dallas Arts District (DAD) partnered with Downtown Dallas, Inc., an organization that stewards and represents downtown Dallas. DAD assumed the responsibilities of the former Arts District Alliance, created in 1984 as Arts District Friends, dedicated to supporting its stakeholders through collaborative events, and marketing to local, regional, national, and international patrons through a redesigned website and intense social media engagement. As the largest contiguous urban arts district in the nation, spanning 68 acres and 19 contiguous blocks, the District's mission is

to enhance the value of the city's creative and economic life by engaging artistic, educational, and commercial neighbors through excellent design, practices, and programs. Aspiring to transform the Dallas Arts District into a dynamic destination for Dallasites and tourists, powered by the imagination of regional and international artists, with integrated and exemplary artistic, residential, cultural, and commercial life, the Arts District was one of only five Dallas destinations to be awarded a maximum 3-star ranking by the coveted Le Guide Vert Michelin Green Guide. Dallas Arts District Foundation stimulates local economic and cultural life through micro grants that bring innovative and new works to the neighborhood and has distributed over 450 grants totaling \$1.2 million since 1990.

RESOLUTION OF ADOPTION/LETTER FROM THE CHAIR

DRAFT

DALLAS ARTS DISTRICT INFRASTRUCTURE COMMITTEE

Jill Magnuson, Chair*

Nasher Sculpture Center

Doug Curtis, Chair Emeritus*

AT&T Performing Arts Center

Father Stephen Bierschenk

Cathedral Shrine of the Virgin Guadalupe

Brent Brown (2015-2017)

City Design Studio

Abraham Carrillo

Crow Collection of Asian Art

Zenetta Drew

Dallas Black Dance Theatre

Brittney Dubose^o

City of Dallas, Office of Cultural Affairs

Father Rudy Garcia (2015-2017)

Cathedral Shrine of the Virgin of Guadalupe

Tara Green (2015-2017)

Klyde Warren Park

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Dallas Arts District

Dan Kohl^o

Perot Museum of Nature and Science

Lea Anne Laughlin

Hall Group

Ramsey March

Stream Realty

Kevin Moriarty

Dallas Theater Center

Debi Peña

Dallas Symphony Orchestra

Lucilo A. Peña*

One Arts Plaza

Kit Sawers (2018-2020)

Klyde Warren Park

Ann Spain (2018-2020)

2100 Ross

Mike Spiewak

Perot Museum of Nature & Science

Dr. Andy Stoker

First United Methodist Church

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EXECUTIVE SUMMARY

“[We are] committed to maximizing the economic and cultural potential of the Dallas urban Arts District... Art and culture are important to our city’s soul and economic vitality.”

Mayor Mike Rawlings

PURPOSE & USE

The purpose of the Dallas Arts District Connect Master Plan is to guide the design and construction of new and replacement features in the Dallas Arts District. As an urban, mixed-use neighborhood, changes to the Arts District should support the five pillars of its mission: Collaboration, Education, Community, Inclusion, and Innovation. This Plan consciously lays out a vision for the future of the Arts District and its built environment. The Infrastructure Committee of the Dallas Arts District has acted as a steering committee for developing the Connect plan and has served as a voluntary review committee of projects in the Arts District.

The Connect plan is intended to guide any designer, decision maker, or developer proposing changes in the Arts District and thus ensure that any such proposal supports the Arts District's goals.



VISION FOR THE DALLAS ARTS DISTRICT

The Dallas Arts District is an urban cultural district that creates a vibrant, healthy, and complete neighborhood for generations to come.

Having achieved excellence in both institutional and architectural accomplishment, the District must now continue to reach for equally exemplary urban experience. Indeed, it must achieve the full city-making ambitions of the original Sasaki Plan.



The Connect plan will:

- 1) Enable additional uses, including housing, to join an expanded Arts District boundary to create a more bustling, livable, walkable, and complete neighborhood;
- 2) Improve the connections between the Arts District and downtown neighborhoods via streetscape improvements, landscape design, transit, and public art;
- 3) Update urban design and development guidelines for the Arts District and the areas immediately adjoining to make certain that the quality and urban character of the private investment matches that of the cultural institutions themselves;
- 4) Increase the number of informal interactions so that more Dallas citizens are attracted to participate in the experiences, performances, events, and personality of the District.

The Connect plan also establishes Design Guidelines for the Dallas Arts District. The Design Guidelines should be used by designers to create a project that embodies the 5 Key Strategies, in support of the Vision for the Dallas Arts District contained in this document. The Design Guidelines are intended to:

- Create a cohesive District identity and character;
- Prevent design decisions that result in negative social and environmental impacts;
- Enable coordination between private developers and the City;
- Coordinate all planning efforts with Downtown Dallas, Inc.; and
- Provide concrete principles against which a project can be judged.

Opposite
The Dallas Arts
District Block Party on
Flora Street in 2017





The University District



KEY STRATEGIES

There are five Key Strategies that guide the Plan to achieve its stated vision. These strategies are the result of an extensive engagement process with Arts District stakeholders over the course of more than a year of research and analysis, interviews, and outreach events.

- 1** Transform Pearl Street into the “Avenue to the Arts.”
- 2** Reinvigorate Flora Street/Ann Williams Way as the cultural core of the Arts District.
- 3** Embrace Ross Avenue as a mixed-use commercial corridor.
- 4** Expand and update wayfinding, signage, public art, and gateway experiences.
- 5** Enhance pedestrian connections in all directions, with a focus to the west. (Perot Museum, West End, and Victory Park)

DESIGN GUIDELINES

The design guidelines provided in the Plan fall into four over-arching categories: Sidewalks, Traffic Calming, Development Controls, and Public Art. Sidewalk design guidelines govern the pedestrian experience on the curb, between vehicle lanes and buildings. Traffic Calming guidelines govern the dimensions and uses of the roadways themselves, including pedestrian crossings. Development Control guidelines govern the architectural and zoning features of structures on private property. Public Art guidelines govern the selection, placement, and installation of art in the public realm within the Arts District. Several design requirements and recommendations will apply throughout the District; six Street Typologies provide more specific requirements and recommendations based on the particular character of various types of streets in the District.

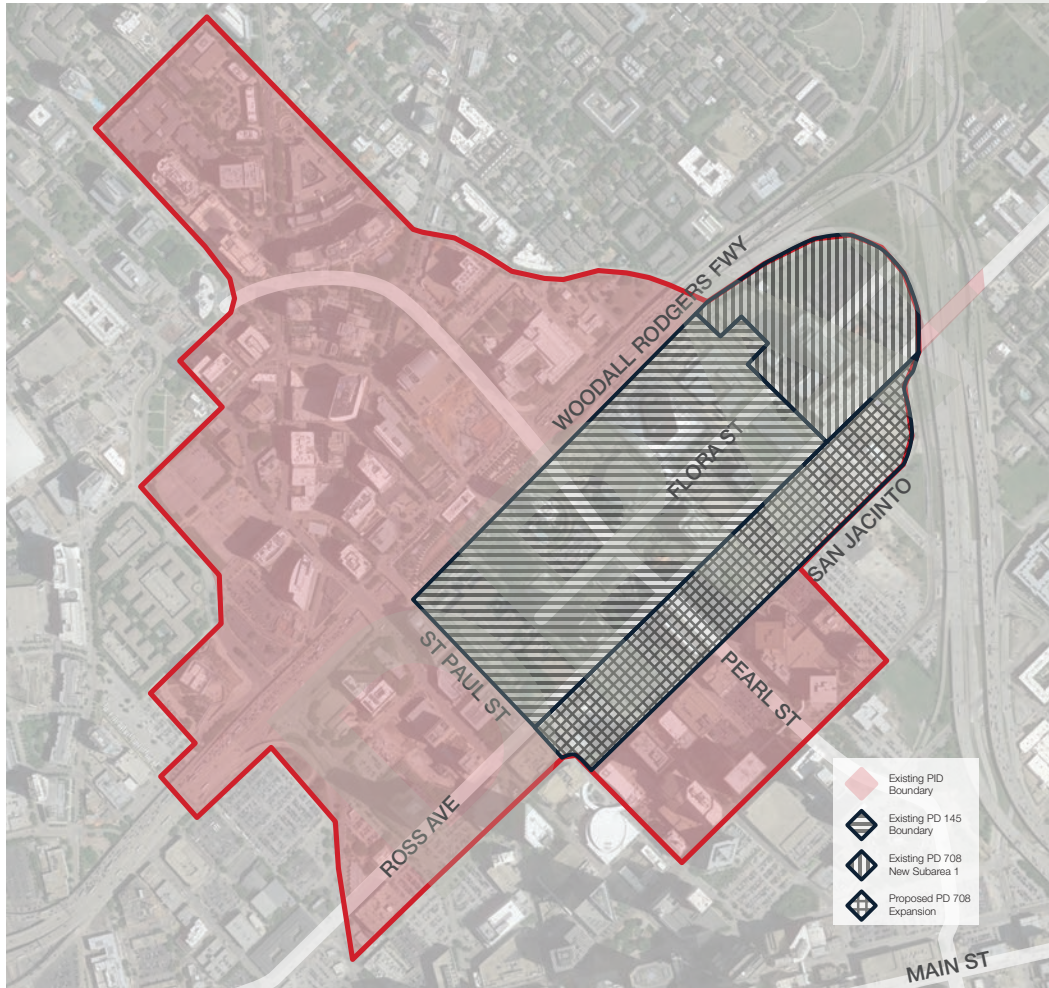
STREET TYPOLOGIES

The six street typologies are intended to provide a generalized categorization of all of the streets relevant to the Arts District. The typologies provide a framework by which to understand the purposes different streets serve.

The six typologies are:

- 1) Pearl Street: "Avenue to the Arts"
- 2) Flora Street/Ann Williams Way: Mixed-Use Cultural Corridor
- 3) Ross Avenue: Mixed-Use Commercial Corridor
- 4) Pedestrian-Priority Streets
- 5) District Connectors
- 6) Service-Oriented Streets





Opposite

A map of all street typologies that are relevant to the Connect Master Plan. All illustrated typologies fall within the existing KWP/DAD PID boundary but go beyond the existing DAD boundary; this is in recognition of the need for strong interconnections between the District and its surrounding neighborhoods.

Left

A map of the original extent of the Arts District and its expansion across Ross Avenue, relative to the KWP/DAD PID boundary.

By many measures, the Dallas Arts District has surpassed all expectations: emerging as perhaps the most pre-eminent arts district in the nation, featuring, in one place, over 15 cultural institutions of local, national, and international significance. Flora Street has indeed become the physical and thematic link between the institutions lining both sides of the street.

INTRODUCTION & PLANNING CONTEXT

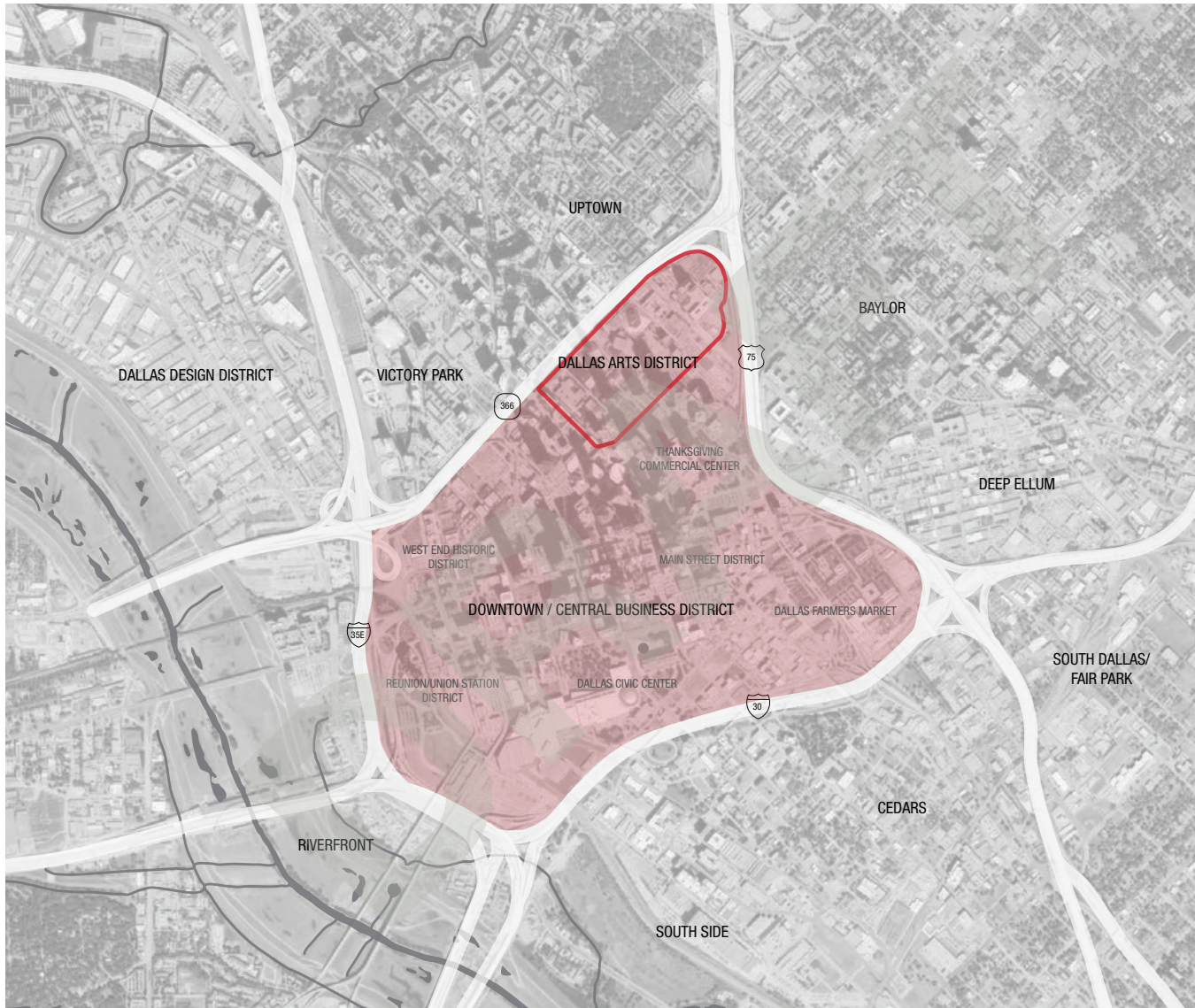
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PURPOSE & USE

At its most fundamental level, the purpose of the Dallas Arts District Connect Master Plan is to provide guidance for design and construction of structures, landscapes, public art, and streets within the legally defined boundary of the Dallas Arts District. The Connect plan is intended to provide a vision as guidance for urban design review of projects, specific intentions for each street within the Dallas Arts District, and suggestions and recommendations for best practices, to which all projects in the Dallas Arts District are encouraged to adhere.

It is recommended that developers, designers, City staff, and other key Arts District stakeholder groups that may influence its built environment all read and fully understand the details of the Dallas Arts District Connect Master Plan. A thorough reading of the plan will help ensure that any proposed change to the Arts District's physical character supports the vision set out in this document.

Opposite
The Dallas Arts District in the context of greater Downtown Dallas.



THE SASAKI PLAN IN RETROSPECT

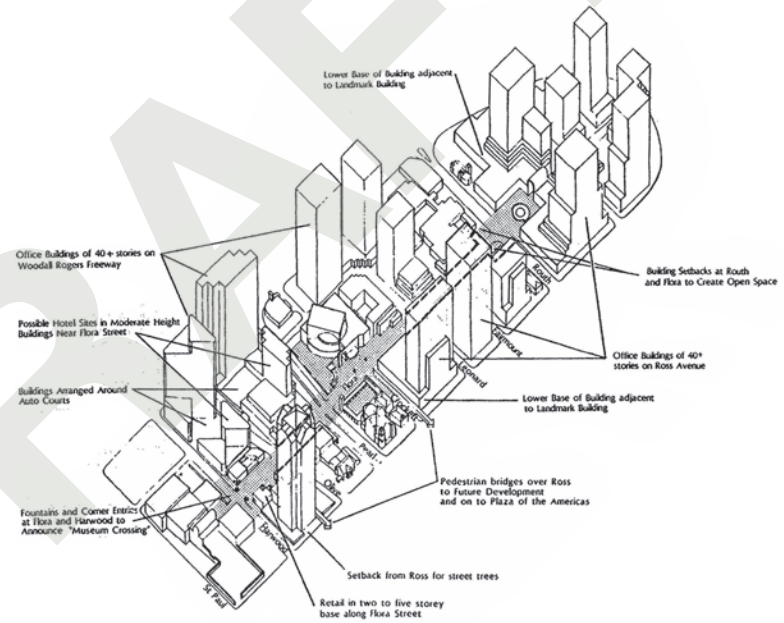
When the Sasaki Plan was published in 1983, the nascent Arts District Consortium suggested the following objective: to create an arts district “to reflect a multi-national atmosphere, and contain mixed uses—arts facilities, office, retail and residential spaces, and cultural events—and feature Flora Street as a physical and visual link within the district.”

By many measures, the Dallas Arts District has surpassed all expectations: emerging as perhaps the most pre-eminent arts district in the nation, featuring over 15 cultural institutions of local, national and international significance in one place. Flora Street has become the physical and thematic link between the institutions lining both sides of the street, beginning at the Dallas Museum of Art (DMA) and continuing beyond One Arts Plaza, a mixed-use complex, to the Dallas Black Dance Theatre and the northern end of Ann Williams Way.

The Dallas Arts District Urban Design Plan, completed in 1983 and referred to as the “Sasaki Plan” after the firm that drafted it, outlined an ambitious vision for 12 square blocks of nearly vacant land on the northern edge of downtown. The Sasaki Plan called for a mixture of activities and land uses to ensure “a steady stream of activity.” Land uses anticipated cultural institutions, a few of which had already been built, but also large amounts of retail, restaurant office, and residential uses lining Flora Street. The plan envisioned three thematic clusters: galleries and boutiques, theaters and a cinema, and cafés and gourmet food, respectively. A study estimated demand for over 213,000 square feet of food services and another 292,000 square feet of retail services in the district at full build-out by the year 2000.

A Pedestrian Network component to the plan proposed extensions to the city’s

then-popular skywalk system over Ross Avenue and suggested a below-grade crossing under busy Pearl Street to avoid conflicts with vehicles. The plan featured specific design guidelines for Flora Street as a major pedestrian corridor and connection between the district's various uses. The plan envisioned two rows of trees on all streets in the district, with three rows on Flora Street. “Small scale modular pavers” were proposed for the tree zones with a narrow “window shopping path” adjacent to building frontages. Pedestrian-scale lighting on Flora Street was proposed to be attached to tree guards and augmented by continuous bollard lights, while Pearl, Ross, and Woodall Rodgers were to be illuminated by larger “Boulevard” fixtures necessary for wider streets. Paired gateposts were proposed along Flora Street to mark district gateways.



Sasaki Plan, Building & Massing Diagram

Development guidelines in the Sasaki Plan included requirements for ground level transparency, setbacks for buildings over two stories along Flora Street and the placement of vehicular driveways on side streets away from pedestrian zones. The guidelines recommended building materials that “reflect regional character,” including “stone, stucco, concrete in light earth tones.”

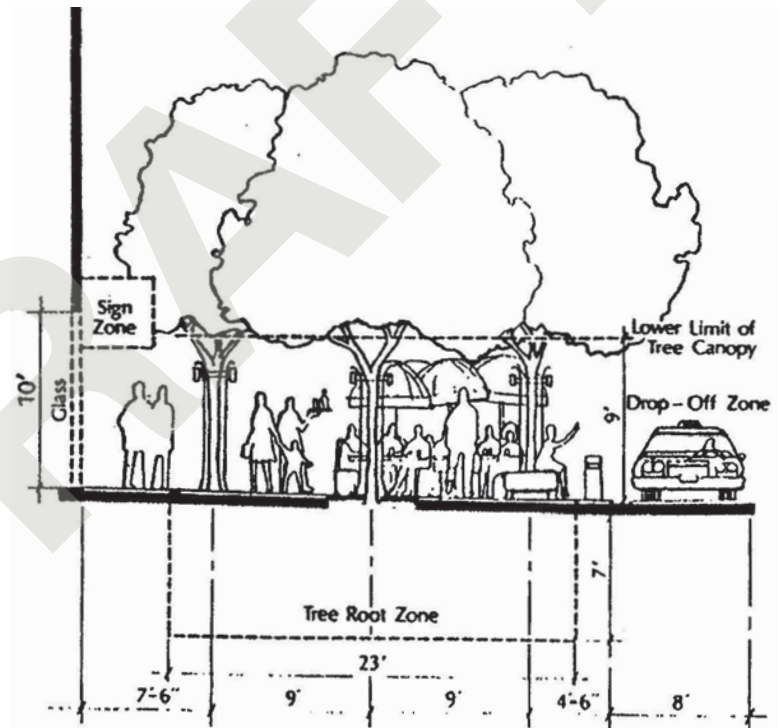
The Sasaki Plan proposed two methods for discretionary review. The first suggested the creation of a district-specific Design Review Committee to evaluate projects for development impacts on the district and compliance with the Connect plan. The second suggested the city create a Planned Development District to review procedures typical of such designation. The City eventually adopted the latter mechanism by creating two Planned Development Districts with all projects

reviewed by the City Department of Environmental Services.

The plan was adopted into zoning, and the Design/Development Guidelines have been applied to subsequent projects in the district and have directed public infrastructure funding for 35 years. Today, the Dallas Arts District is approaching full build-out: only a few parcels remain for development, and many of the City’s most cherished cultural venues have found their way to District. The most recent additions to Dallas’s cultural scene, such as the Perot Museum of Nature and Science, have had to find a home at the periphery of the original District boundaries. Meanwhile, the standards established in 1982 have largely fulfilled their original intent to create a unified district, with infrastructure standards that set a high bar for the quality of Flora Street as the focus for the arts. After nearly 35 years, mixed-use

facilities are now finding a home in the Arts District, with destination restaurants along Flora Street and hundreds of residential units built and more in the pipeline.

District-Wide Concepts and the Urban Design Plan sections of the Sasaki will be replaced with a new vision for the district in the 21st century. The former recommendations for Food Service, Retail and Arts Program will not be replaced in the new plan, because the Dallas Arts District, a registered 501(c)3 organization, now actively supports arts programming in the District and advocates for retail and food uses that are compatible with the district identity and quality. The changing nature of Dallas development has eliminated the need for such specific use recommendations, nor were such plan recommendations effective in the past. The Dallas Arts District has matured as an organization, and actively promotes both



Sasaki Plan,
Flora Street section

arts programming in the District and the interests of its stakeholders.

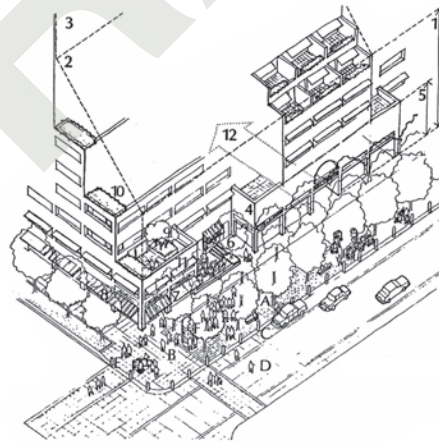
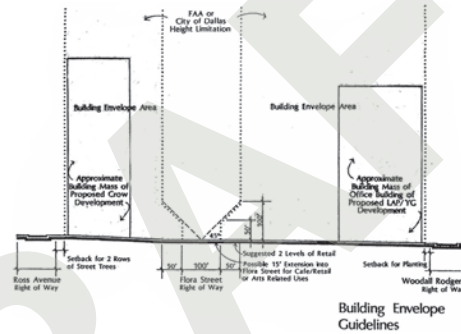
Several elements of the Design/Development Guidelines have simply outlived their original intent, while others have not stood the test of time. The standards for trees and paving on Flora Street, for example, have proven to be problematic for maintenance, universal access, and outdoor retail uses. As a result, most of the projects approved in the last decade have requested, and been granted, changes to both of these standards. Other development standards from 1982, such as the recommended street lighting, benches and street paving specify outdated technologies and need to be updated with current standards. The street lighting, for example, needs to be converted to LED fixtures and the thin stone pavers at street crossings are a regular maintenance problem for Public

Works.

The regulation of building heights, particularly the regulation on Flora Street, has been implemented and has resulted in a pleasant low-scale feeling that should be preserved. Likewise, requirements for building transparency of the ground floor have been implemented with good results at street level; these regulations will be preserved in the new zoning scheme.

Perhaps the most problematic element of the Sasaki Plan is related not to the plan recommendations themselves, but to the process that was adopted to regulate and enforce the plan provisions. The current procedure for review of projects within the Dallas Arts District is identified in the Planned Development (PD) regulations that call for Development Plan review for all development within the Dallas Arts District. This section will be replaced

with more flexible zoning standards that meet the goals of this plan. It is expected that the updated zoning standards will strengthen and improve the building and zoning permit approval process and avoid the frequent use of the special exception process through the Board of Adjustment. Nearly every project in the past decade has had to resort to this time consuming, inconsistent process to seek exception from the outdated regulations in the Sasaki Plan.



Sasaki Plan,
Guidelines Diagrams

EXISTING CONDITIONS ANALYSIS & CONTEXT

Dallas Planning Context

DOWNTOWN DALLAS 360

Over the past 6 years, Downtown Dallas, Inc. (DDI) has undertaken an ambitious planning effort to create and implement a vision for Downtown. Starting with a plan for the area within the innermost highway belt in 2011, called Downtown Dallas 360 (“the 360 Plan”), the push for a broader look at Downtown led to the 2017 Downtown Dallas 360 Plan update.

The 2011 360 Plan established a vision for Downtown, which is presented as:

“Downtown Dallas is a complete urban center composed of distinct yet interconnected districts linked by an accessible transit network, each offering a unique and diverse combination of places to live, refreshing open spaces, bustling street

activity, successful business and retail, and dynamic urban experiences for residents, workers and visitors alike.”

The intention of that document was to remain “nimble”, consistently adapting to new conditions and address any potential future concerns. In that spirit, the 2017 360 Plan update expands on the original plan to include districts to the north, east, and south of the original plan area in recognition of the physical and social connections with those districts that are necessary to ensure the long-term success of Downtown.

The Dallas Arts District has always been a key district in the Downtown Dallas 360 planning process, and with the 2017 update reflects the importance of the Arts District to Downtown, the alignment of DDI's goals for Downtown with those of the Arts District, and the capacity of

the Arts District and its team to provide guidance on the 360 Plan for the Arts District and its immediate surroundings

KLYDE WARREN PARK

Since its opening in 2012, Klyde Warren Park has had a transformative impact on Dallasites' conception of what an urban space can provide. Its robust programming, high-quality design, and ability to ameliorate the harsh impacts of automobile infrastructure have combined to bring a diverse audience to Downtown and experience it in a way they previously could not imagine.

The impacts extend beyond the park's boundaries: new development and existing institutions such as the DMA are orienting themselves toward the park and looking beyond their walls to connect to the larger Downtown. The Dallas Arts

District celebrates the positive effects of the park on ground-level pedestrian experiences, development quality, and perceptions of Downtown, and seeks to continue those trends.

DOWNTOWN & SURROUNDING DEVELOPMENT PRESSURES

While Dallas' real estate market continues to demonstrate remarkable growth as of 2020, not all districts are rising at the same rate. Much of Downtown Dallas' existing office and retail spaces were built with an earlier idea of urban life and met tenant demands that no longer hold. New development that more closely represents tenant needs in Uptown has shifted the center of gravity for those spaces: per square-foot rental rates in Uptown now far outpace those in Downtown.

At the same time, a growing residential population in Downtown has dramatically changed the urban environment there. According to Downtown Dallas, Inc. (DDI), between 2002 and 2020, the population of Downtown was estimated to have increased from a mere 200 to more than 12,600 residents. This increase of over 6,300 percent increase has helped usher in a comparative explosion in retail, restaurant, and public open spaces. Downtown is progressively becoming an 18-hour district with the potential to be a 24-hour district, rather than the 8-hour district it has been for much of its history.

Arts District Development Patterns

The Dallas Arts District was once a critical center of Dallas' African American population as part of the North Dallas and State-Thomas neighborhoods. Densely

organized single-family homes helped create a tight urban fabric that supported schools, local businesses, and community hubs such as the Moorland YMCA – now home to the Dallas Black Dance Theatre – on what is now Ann Williams Way, formerly Flora Street, in the Arts District.

Decades of disinvestment due to red-lining, institutionalized segregation, and the construction of the Woodall Rodgers and I-345 freeways left North Dallas and State-Thomas depleted and disconnected. The community and its urban fabric suffered from high vacancy and crumbling infrastructure as a direct result of these actions. By the time the original plan for the Arts District was finished in 1983, the area was largely parking and auto sales lots.

For the first 20+ years of its existence as the Arts District, the land bounded by Woodall

Rodgers, I-345, Ross Ave., and St. Paul St. fully embraced its new moniker: with the exceptions of Trammell Crow Center, every new structure built or renovated in the Arts District was an arts venue: DMA, Meyerson Symphony Center, Nasher Sculpture Center, Dallas Theater Center, Dallas Black Dance Theatre, and AT&T Performing Arts Center were all completed by 2009, with the Moody Performance Hall coming shortly thereafter. These venues, though iconic and architecturally impressive, helped create a discontinuous district with a minimal mix of uses that resulted in periods of near-emptiness punctuated by occasional large events.

The Dallas Arts District has only recently begun to experience the same diversity and intensity of use that was always anticipated by the original vision for the District, and that the rest of Downtown is also undergoing. The completion of One

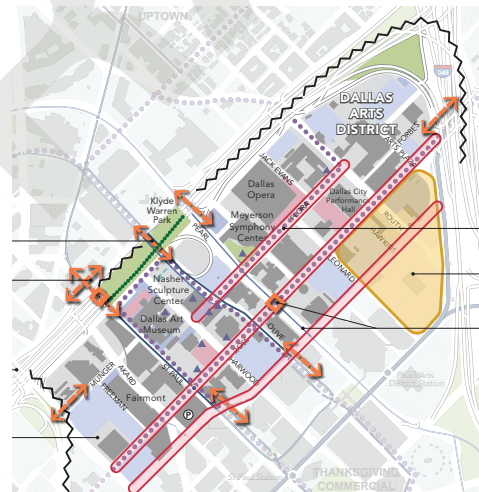
OPPORTUNITY

To achieve a more urbane Dallas Arts District means forging even better connections to adjoining districts and other nearby neighborhoods. It means finding a way to insert more of the mixture of uses that were initially imagined, and it means facilitating a broader array of programming and activities when concerts or openings are taking place—but more importantly when no special events are occurring in the cultural venues.

The *Dallas Arts District Master Plan* proposes a new set of principles to guide it for the next three decades during which the Arts District will rise to its own earliest goals, and to the City of Dallas' current civic ambitions. The master plan will update the guidelines from the *Sasaki Plan*, preserving those standards that have served the district well, while replacing those that have proven to be problematic or

outdated. The *Dallas Arts District Master Plan* vision is driven by five reinforcing strategies that will expand the impact of the District and improve connectivity to adjacent neighborhoods.

1. Transform Pearl Street into the "Avenue to the Arts".
2. Reinvigorate Flora Street as the cultural core of the Arts District.
3. Embrace Ross Avenue as a mixed-use commercial corridor.
4. Expand and update wayfinding, signage, public art, and gateway experiences.
5. Enhance pedestrian connections in all directions, with a focus to the west (Perot Museum, West End, and Victory Park).

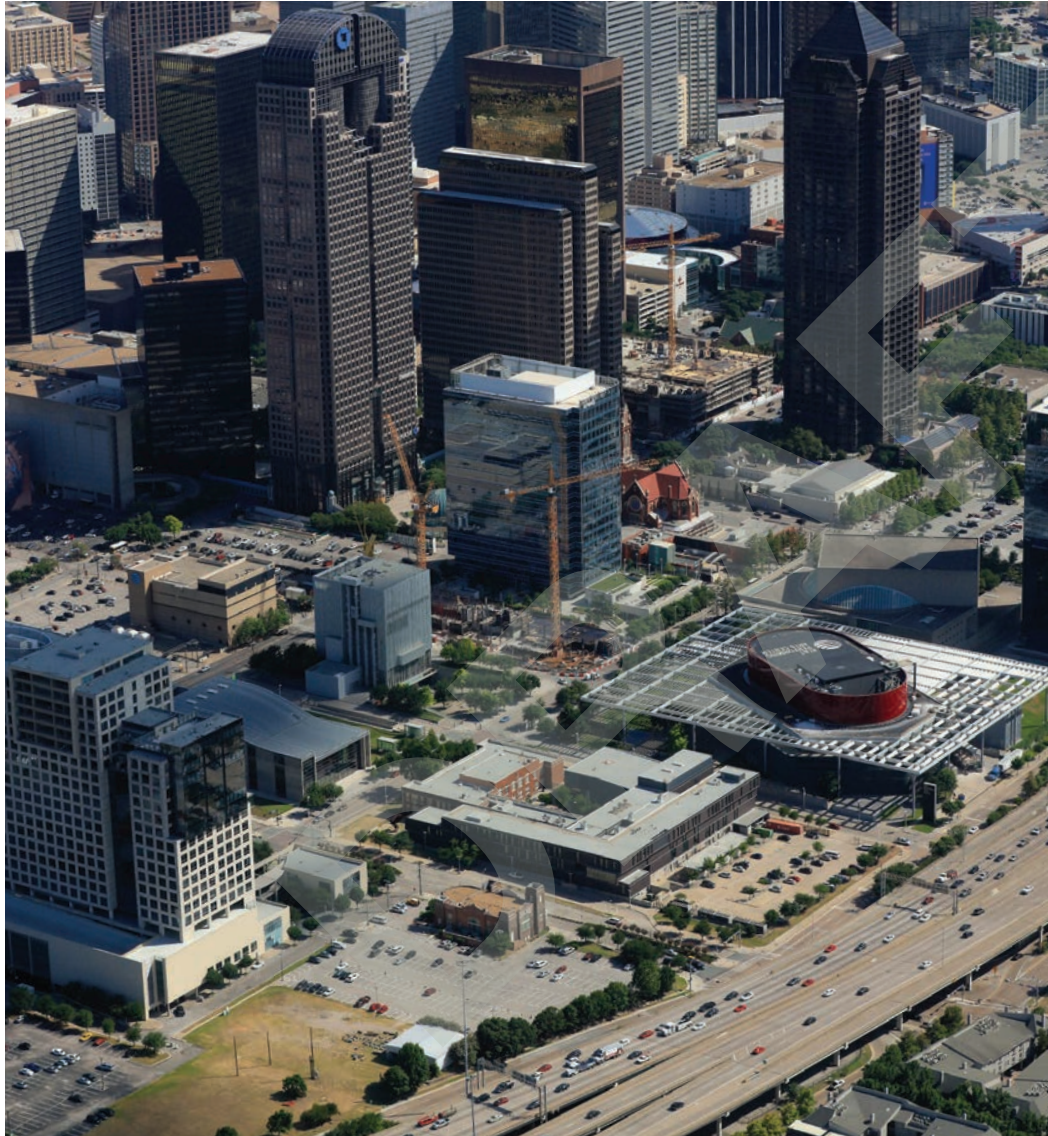


Above
Key areas of opportunity for the Dallas Arts District identified in the 2017 Downtown Dallas 360 Plan by Downtown Dallas, Inc.

Right
2017 Downtown Dallas 360 Plan granular assessment of Dallas Arts District, identified in the plan as a Catalytic Development Area.



Right
View of Dallas Arts
District in 1989



Left
View of Dallas Arts
District in 2018

Arts Plaza, Hall Arts phase I, Museum Tower, Arts Apartments, 1900 Pearl, and the progress of Hall Arts phase II bring a much-needed mix of new uses to the District. The renovation of the Trammell Crow Center into a new mixed-use office building and construction of the Hall Arts Phase II hotel and residences reflect the increasing investment in Downtown and growing interest in creating a dynamic, mixed-use neighborhood in the Arts District. However, the current uses and physical environment do not yet realize the vision of the Arts District as a vibrant urban district.

Opposite and Right
Examples of development projects underway in the Arts District at the time of printing.

Opposite
Trammell Crow Center 2.0 (Stream Realty) (top). Hall Arts Phase I (bottom left and center) and II (bottom right).

Right
Atelier Flora Lofts (Ogiesby Greene)





Arts District Infrastructure

Though envisioned as an oasis amid the hustle and bustle of Downtown, the physical infrastructure installed in the Dallas Arts District aimed at achieving that atmosphere has aged significantly and fallen short of its original ambitions.

The following conditions have resulted from a mix of under-investment and the lack of application of best practices:

- Unstable sidewalk pavers have created unsafe pedestrian walkways;
- Inappropriate plantings in under-engineered planting beds have not survived;
- The extreme density of a triple row of trees along Flora St. has made new development difficult and limited desirable sidewalk uses;

- A district-wide lighting scheme that prioritizes automobiles over pedestrians has made nighttime experiences feel uncomfortable and unsafe;
- Roadway pavers unsuited to actual traffic volumes and speeds have created dangerous pedestrian environments, with concrete being ejected from the roadway into pedestrian zones at the intersection of Flora and Pearl Streets;
- Inconsistent wayfinding and signage fails to provide adequate guidance and does not reflect the desired character of the District; and
- Surface conditions are inconsistent with American Disability Act Standards for Accessible Design to ensure universal access to streets, sidewalks, and other critical public infrastructure in the District.

Opposite

Examples of existing conditions in the Arts District. Current infrastructure suffers from deferred maintenance and design that reflects outdated concepts of urbanism.



COMMUNITY ENGAGEMENT PROCESS

In recognition of the role that the Dallas Arts District plays as a crossroads for many existing neighborhoods and communities, the Arts District has undertaken an extensive public and community engagement effort. The process, which took over four years in total, brought as many stakeholders to the table as possible to generate a vision for the Dallas Arts District. Several group meetings were held over the past four years (Appendix II, pp. 142-145). Because of the Arts District's desire to serve as wide an audience as possible, special attention has been given to the needs of those arriving by alternative modes of transportation, such as public transit, walking, and bicycle.

Early in the planning process, the project team began a series of in-depth interviews with key stakeholders affiliated with the Arts District. Starting in January 2016 and

continuing through the remainder of 2016 and into 2017, NBBJ and Arts District staff members scheduled and conducted individual and small group conversations. These included active members of the Dallas Arts District, as well as property owners, institutional administrators, real estate developers, City staff, and neighborhood organizations whose activities are likely to be impacted by or influence those of the Arts District. Through these interviews, stakeholders clarified their top priorities, concerns, and plans for their own organizations as well as for the Arts District, which have all helped educate the project team and inform the vision for the plan.

Regular coordination meetings between the project team and other relevant planning entities and organizations have been critical to the Arts District planning process. Frequent exchanges of

information and sharing of resources with groups such as Downtown Dallas, Inc.; Uptown, Inc.; and Texas Trees Foundation (TTF) have resulted in a more thorough understanding of the context in which the Dallas Arts District Connect Master Plan will be implemented.

Though the public hearing process for the Arts District's proposed zoning changes requires two public hearings, the Dallas Arts District has always had the intention of holding multiple public, community, and smaller group meetings throughout the planning process. On November 7th, 2016, the project team presented its initial findings and general intentions for the Dallas Arts District Connect Master Plan to the Dallas City Council Arts, Culture, and Libraries Committee. This was the City's first formal introduction to the plan's motivating principles and general timeline. Though many stakeholders had already



had an opportunity to speak with the project team, on February 27th, 2017, the Dallas Arts District hosted a stakeholder forum at the Booker T. Washington High School for the Visual and Performing Arts. During that forum, the project team presented its analysis in addition to the updated set of Key Strategies and project timeline. Several group meetings were held over the past four years (Appendix II, pp. 142-145).

Stakeholder
engagement,
begun winter 2016

PLAN FRAMEWORK

Achieving a more welcoming and accessible Dallas Arts District means forging even better connections to adjoining districts such as Downtown, Uptown, Victory Park, West End Historic District, Deep Ellum, Klyde Warren Park, Pearl/Arts District DART Station and other nearby neighborhoods. It may mean enlarging on the nature of culture so that a wide constituency of citizens feels attracted to and comfortable visiting this urban environment.

VISION

In the early 1980s, Dallas city leaders came together to imagine and build a world-class cultural district. The plan then produced, known as the Sasaki Plan, became and has remained the roadmap for the Dallas Arts District (DAD). The now-mature Dallas Arts District holds the distinction of being the largest contiguous urban arts district in the nation. It is internationally recognized for its outstanding assemblage of cultural institutions that is enabling Dallas to compete among the great cities of the world.

Institutions and organizations in the District include:

- Anita N. Martinez Ballet Folklorico
- Avant Chamber Ballet
- Bruce Wood Dance
- Chamber Music International
- Dallas Black Dance Theatre

- Dallas Chamber Symphony
- Dallas Symphony Orchestra
- Dallas Theater Center
- Fine Arts Chamber Players
- Greater Dallas Youth Orchestra
- Metropolitan Winds
- New Texas Symphony Orchestra
- Oral Fixation
- Texas Ballet Theater
- The Dallas Opera
- TITAS Presents
- Turtle Creek Chorale

Venues in the District include:

- AT&T Performing Arts Center
 - Margot and Bill Winspear Opera House
 - Dee and Charles Wylie Theater
 - Annette Strauss Square

- Hamon Hall
- Elaine D. and Charles A. Sammons Park at Winspear Opera House
- Booker T. Washington High School for the Performing and Visual Arts
 - Montgomery Arts Theater
 - Diane and Hal Brierley Experimental Theater
 - M.R. and Evelyn Hudson Gallery
- Cathedral Shrine of the Virgin Guadalupe
- Crow Museum of Asian Art
- Dallas Black Dance Theatre
- Dallas Museum of Art
 - Horchow Auditorium
- First United Methodist Church
- Meyerson Symphony Center
- Moody Performance Hall
- Nasher Sculpture Center

- Perot Museum of Nature and Science
- St. Paul United Methodist Church

Multiple other local groups perform in the Arts District on an ongoing basis, as do visiting national and international cultural organizations. The Arts District has five cultural venues designed by Pritzker award-winning architects, the largest such concentration in one urban district.

Yes, it has been quite an achievement for the City of Dallas. However, having reached institutional and architectural excellence, the District must now continue to reach and achieve an equally exemplary urbanity. Indeed, it must achieve the full city-making ambitions of the original Sasaki Plan.

As downtown Dallas has become more active and vibrant, more diverse, more resident accommodating, and now contains many of the amenities that are

expected of a great downtown, the DAD has to achieve an equally exemplary urbanity. The Downtown Dallas 360 Plan speaks to this, promising the “Building a True Urban Experience” throughout the downtown.

Achieving a more welcoming and accessible Dallas Arts District requires forging even better connections to adjoining districts such as Downtown, Uptown, Victory Park, West End Historic District, Deep Ellum, Klyde Warren Park, Pearl/Arts District DART Station and other nearby neighborhoods. The Arts District must also find a way to insert more of the mixture of uses that was initially imagined, facilitating a broader array of programming and activities when concerts or openings are taking place – but more importantly when no large events are occurring in the cultural venues. Achieving such a District may mean enlarging on the nature of culture so that a wider constituency of citizens feels attracted to and comfortable

visiting this urban environment. The place should feel more full than it does – more active, even somewhat crowded, and occupied by a cross-section of Dallas citizens – on all occasions.

Thus the four broad goals of the Connect plan are:

- 1) Enable additional uses to join an expanded Arts District boundary, including housing, to create a more bustling, livable, walkable, and complete downtown neighborhood;
- 2) Improve the connections between the Arts District and downtown neighborhoods, via streetscape improvements, landscape design, transit, and public art;
- 3) Update urban design and development guidelines for areas immediately adjoining the Arts District to make certain that the quality and urban character of the private investment matches that of the cultural institutions themselves;

- 4) Increase the programming of activities so that more Dallas citizens are attracted to participate in the experiences, performances, events, and personality of the environment.

This has been occurring, and this plan is intended to accelerate such an evolution. More than a stage for a certain set of cultural activities, the Dallas Arts District is on its way to becoming a great mixed-use neighborhood, taking equal pride in accommodating educational, religious, leisure venues and opportunities, along with an increasing number of residential, retail and commercial neighbors. It is no longer enough to simply spruce up Flora Street; it is essential to connect the Arts to the broader range of activities and places that create an enjoyable and humane place.

The Dallas Arts District Connect Master Plan proposes a new set of principles that

will guide the DAD for the future, during which the Arts District will rise to its own earliest goals and to the City of Dallas' current civic ambitions. Not least of these ambitions is to continue to enhance the city's public realm. From Klyde Warren Park over Woodall Rodgers Freeway to the Katy Trail, Dallas residents are enjoying public spaces as never before. Downtown hosts many new residents who demand a high-quality connected environment that offers convenience for walking and cycling, not just vehicular mobility.

The plan outlines key strategies for improving and expanding the Arts District beyond its current boundaries. Flora Street will, of course, be maintained as the central spine of the DAD, but requires substantial repairs. Pearl Street and Ross Avenue are identified for significant enhancements. The other streets that run through the DAD will require better

accommodation for pedestrians, including safer pedestrian crossings. The plan also recommends exploring better way of moving art and performances outside the walls of the existing institutions.

The plan also updates the guidelines from the Sasaki Plan to be more environmentally focused, resource conscious and provide universal accessibility. The new guidelines preserve those standards that have served the district well, while replacing those that have proven to be problematic or outdated.

Opposite

The Dallas Arts District & its surroundings, including proposed development.



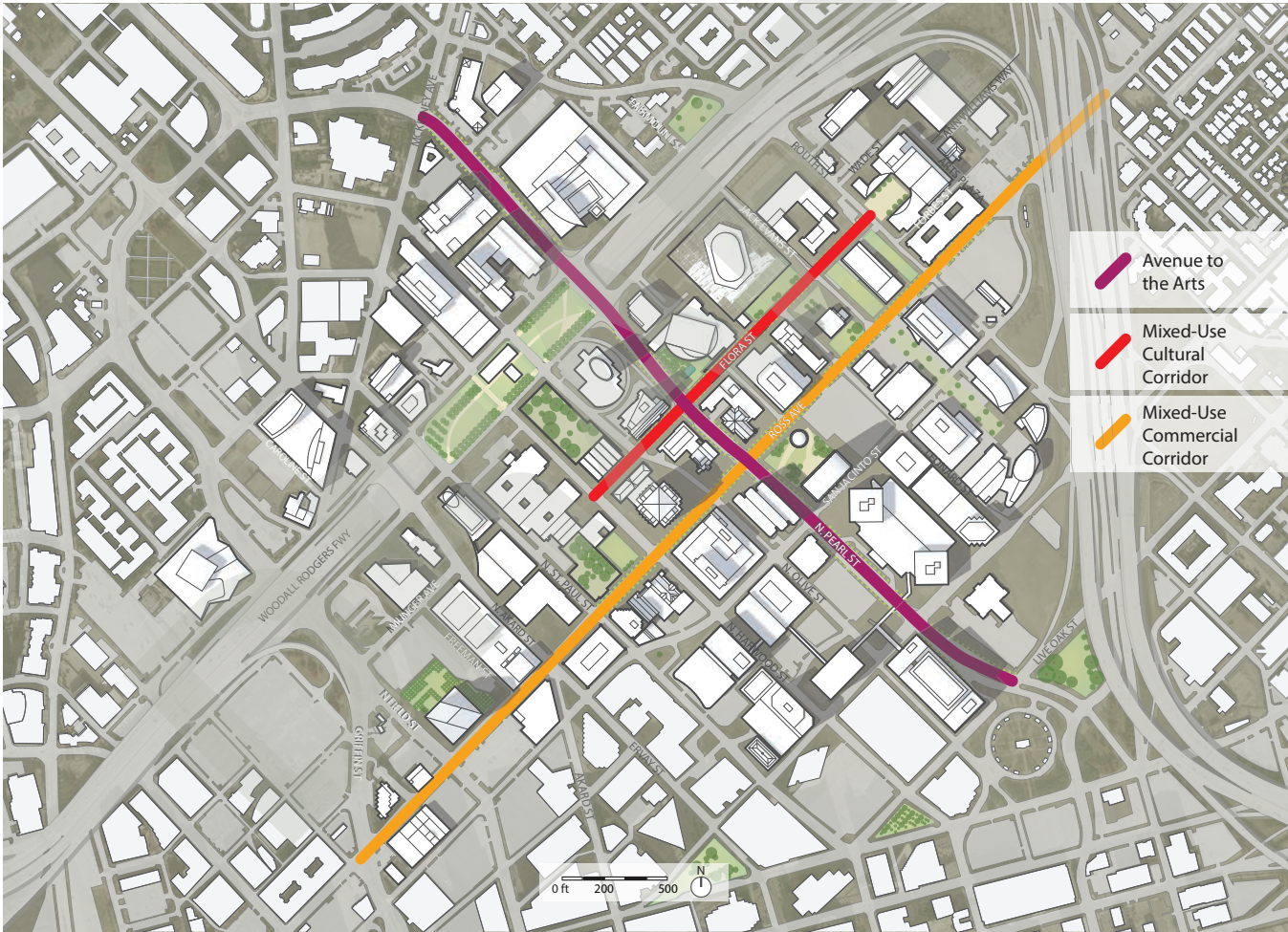
5 KEY STRATEGIES

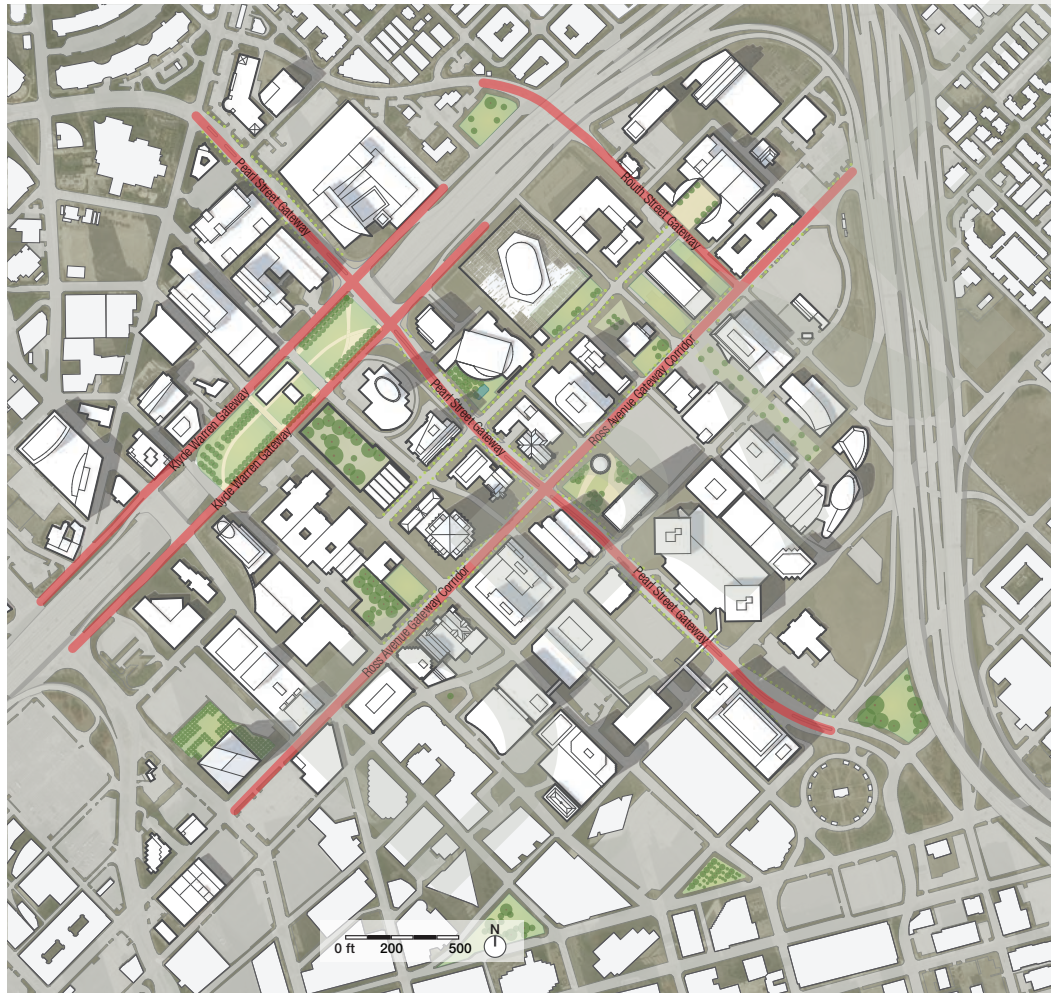
The Dallas Arts District Connect Master Plan vision is driven by five reinforcing Strategies that will expand the impact of the District and improve connectivity to adjacent neighborhoods. The first three Strategies are primarily focused on enhancing three major roadway corridors within the district. Flora Street paving, trees, and lighting will be replaced to provide more contemporary infrastructure for emerging commercial uses within a cultural district. Pearl Street and Ross Avenue will be treated with the same level of design scrutiny and investment in the public realm as Flora Street. Pearl Street will extend the Arts theme between Uptown and Main Street, while Ross Avenue will become a complementary, mixed-use street parallel to Flora Street with the ability to support future transit.

The remaining two Key Strategies are to enhance wayfinding, signage, gateways, and public art and to improve connections to the west. Art works, either temporary or permanent, will announce the district at various gateway points to define a broader conception of the Dallas Arts District. In light of the Perot Museum location west of N. Field, the need to expand public perception of the Arts District should be achieved with strategic placement of public art, enhanced pedestrian crossings and sidewalks, and a contemporary WiFi-enabled public realm to enhance access to users of all economic strata.

The five Strategies are:

- 1** Transform Pearl Street into the “Avenue to the Arts”.
- 2** Reinvigorate Flora Street/Ann Williams Way as the cultural core of the Arts District.
- 3** Embrace Ross Avenue as a mixed-use commercial corridor.
- 4** Expand and update wayfinding, signage, public art, and gateway experiences.
- 5** Enhance pedestrian connections in all directions, with a focus to the west (Perot Museum, West End, and Victory Park).





Left
There are four critical gateways and gateway corridors in the Downtown area that impact the Dallas Arts District. Visitors most frequently encounter the Arts District via these gateways and they often constitute a visitor's first experience of the Arts District.

Opposite
Strategies 1, 2, and 3 focus on improving the major roadways through the Dallas Arts District: Flora Street, Pearl Street, and Ross Avenue.

1

TRANSFORM PEARL STREET INTO THE “AVENUE TO THE ARTS”

After 30 years of focus on Flora Street, the time has come for the Dallas Arts District to extend and connect to the districts and neighborhoods to the north and south. Pearl Street is a large, challenging street, but the ambitions of the Dallas Arts District are likewise large and opportunity-seeking; a vision for the District that opens itself seamlessly to visitors from Uptown and Downtown, and neighborhoods beyond. Pearl Street should no longer be a divider of the Arts District, but rather the focus of a grander vision for the Arts in Dallas. Pearl Street should be the most striking arts corridor in the City, with additional arts, culture, and entertainment venues sprinkled from Uptown to Main Street and to Deep Ellum and the Cedars beyond. Pearl Street should provide a welcoming corridor for DART passengers to reach the cultural and art institutions on Flora Street, and become a street worthy of art or cultural institutions in its own right,

much the way Flora Street has been envisioned. However, Pearl Street, unlike Flora, extends far beyond the Arts District. Pearl Street is defined by the City as a District Connector, but has unique characteristics as it passes through the Arts District. A central median is present for much of that length that could be utilized for public art. For much of its length, Pearl Street has three lanes of traffic in either direction with on-street parking during non-rush hours. As described in the Dallas 360 Plan, Pearl Street has opportunities for infill development, activation strategies, and pedestrian enhancements, including mid-block crossings, landscaping, and improved crosswalks. These enhancements will help achieve the vision of Pearl Street becoming the city’s “Avenue to the Arts” as it proceeds through Downtown and the Main Street District to the Dallas Farmers Market.



Special paving at the intersections of Pearl and Ross, as well as other cross streets, would expand the Dallas Arts District, as would the introduction of Dallas Arts District standard lighting fixtures. Special development design controls on abutters of Pearl Street would ensure that no new vehicular or service access points would be built to interrupt pedestrian flows on the sidewalks. Minimum street tree planting requirements will support healthy tree life and growth and ensure sufficient shading for pedestrian comfort and reduction of urban heat-island effect.

Top Left

Philadelphia's South Broad St., the Avenue of the Arts, is an arts-focused downtown thoroughfare to which Pearl St. should aspire



Top Right

Pearl St. should incorporate public art into the median to support its role as the Avenue to the Arts. Signature artwork like Alice Aycock's Cyclone Twist in New York would help set Pearl St. apart.

Bottom Right

Much like New York City's 6th Avenue is also known as Avenue of the Americas, Pearl St. will also be known as the Avenue to the Arts.



Above
Current view of Pearl Street
from above

Opposite page
Proposed street condition
of Pearl Street, including
public art in the medians, an
improved pedestrian crossing,
and greater physical and visual
access to adjacent sites.





Priority Actions

- Reinterpret Pearl Street as “The Avenue to the Arts” from McKinney Avenue to Live Oak Street.
- Create a connection between Downtown neighborhoods.
- Improve deteriorating and unsafe pedestrian R.O.W. and crossings with a focus on safety and comfort.
- Unify and expand the Dallas Arts District with a continuous streetscape between Uptown, Klyde Warren Park, the Arts District, and Downtown.

Implementation and Partners

Downtown Dallas, Inc.

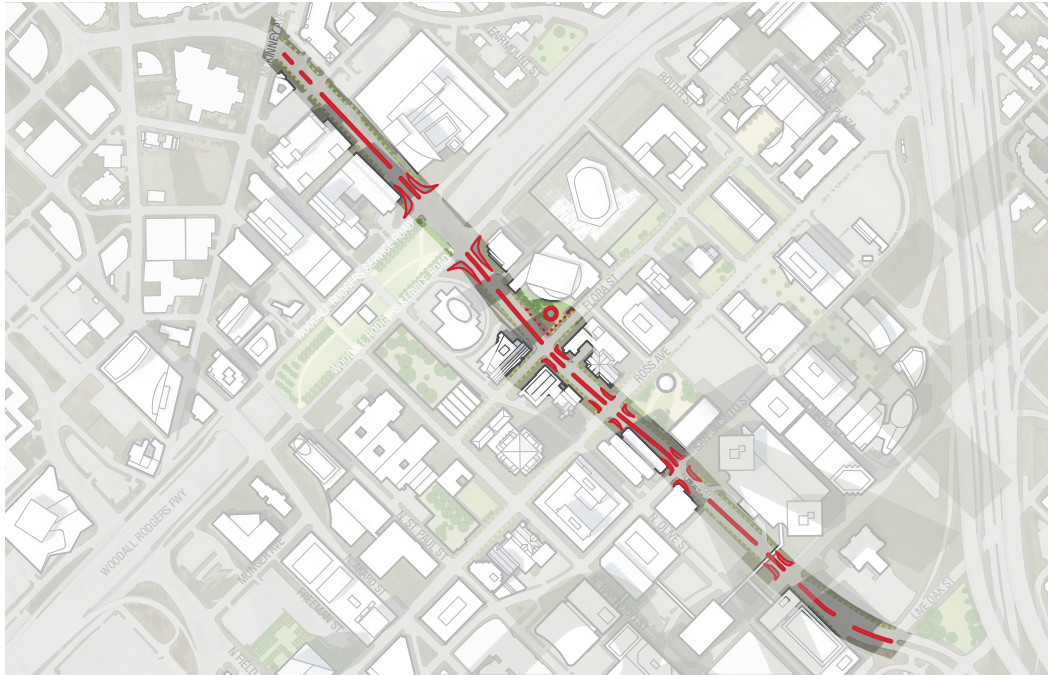
- Advocate for and coordinate Pearl Street lane-reduction process with Dallas 360 thoroughfare recommendations.

City of Dallas

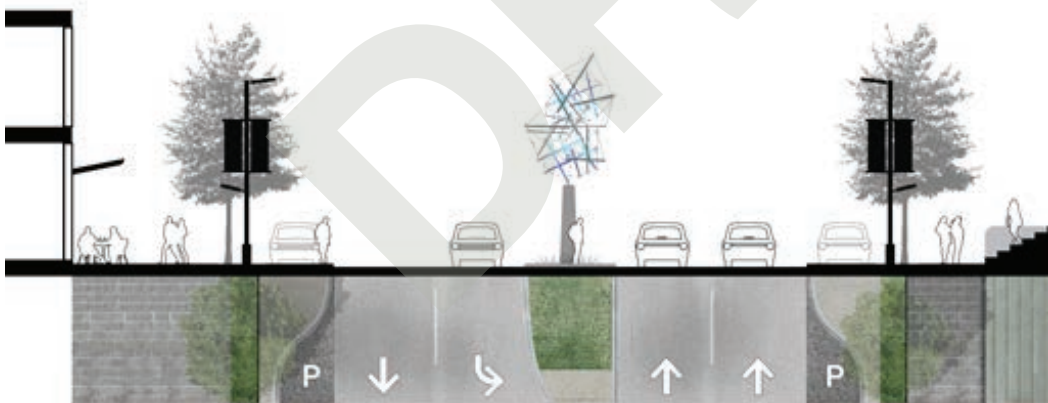
- Approve and implement the Connect plan.
- Advocate for removal of Texas U-Turn at Pearl Street.

Dallas Arts District Actions

- Initiate public art program for temporary and permanent art installations on Pearl Street.
- Initiate tactical, short-term wayfinding at Pearl Street/Arts District DART Station to identify clear routes to Dallas Arts District destinations.
- Advocate for and coordinate Pearl Street improvements with Dallas 360 thoroughfare recommendations.



Top
Conceptual infrastructure changes proposed for the length of Pearl St. between Bryan St. and McKinney Ave.



Bottom
Proposed Street section of Pearl St. at Flora St. The roadway width has been reduced to two travel lanes with curb bump-outs. Blank walls and facades have been replaced with transparency and access, and wider sidewalks are protected from the roadway by street trees.

2

REINVIGORATE FLORA ST./ANN WILLIAMS WAY AS THE CULTURAL CORE OF THE ARTS DISTRICT

Flora Street was designed 35 years ago with a bold vision for a beautiful, tree-lined street with world-class arts institutions, artist housing, retail, and vibrant open spaces strung along its length. While the institutions were realized, many of the other uses never arrived. In the meantime, the aging streetscapes have failed to live up to desires for outdoor programming and universal access. Poor lighting and rampant tree growth have become impediments to retail uses that are just now arriving to re-invigorate the district.

Reinvestment in Flora Street and Ann Williams Way should respect the bold vision of the Sasaki Plan, but design requirements are updated to support the need for outdoor dining, pedestrian traffic, universal access, and new residents. Updated ADA-compliant, paving, energy-efficient lighting, and water-saving landscape standards are essential for

a cohesive, sustainable, and quality experience true to the vision for the Arts District.

For many reasons, tenants and landowners have not embraced the Bald Cypress trees specified for Flora Street. A combination of intrusive root balls, unattractive winter foliage that attracts unwanted birds, and the multiple rows specified in the Sasaki Plan have proven unpopular. As a result, every recent project has petitioned to replace the Bald Cypress with other more desirable species. A variety of attractive trees have been suggested in the new guidelines with the intention of avoiding a monoculture of any single species for the length of the corridor. The Connect plan will reduce the requirement of three rows of trees on Flora to two, as demonstrated by most recent projects.

The original granite cobble paving

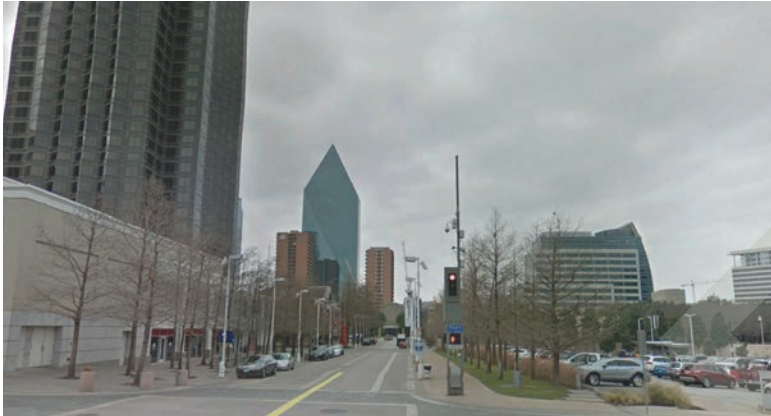
specified on Flora Street has also proven to be problematic for outdoor seating and safe walking. Every recent project in the district has petitioned to replace the paver or alter the installation method to ensure safer pedestrian passage and a more even surface. Other materials, such as stone or concrete pavers, have also been successfully introduced into the district on various projects. The Connect plan will broaden the acceptable paving types as long as the color palate of the original granite pavers is respected.

Flora Street established an extraordinary standard for streetscape design with flush curbs, special street pavers, and pedestrian crossings. These elements should be preserved and repaired with newer more durable materials and installation techniques while preserving the visual quality of the district. Two exceptions are stone bollards and fixed steel benches in the district. The stone bollards are easily damaged and difficult to replace, while the fixed benches are no longer attractive or well-used. The stone bollards will be replaced with stainless

steel bollards at high-traffic areas for pedestrian safety, while the benches will be removed, and it is recommended they be replaced with a program of branded, moveable seating.

The original lighting poles in the Arts District have aged well and will be preserved and extended as the boundary of the district expands across Ross Avenue. Some updating of the fixtures is anticipated, such as the replacement with LED lights to reduce energy consumption. Additional modifications will include lowering of the pedestrian lighting component to ensure adequate light levels below the tree canopy. Many streets in the district that currently lack pedestrian level lighting will benefit from the introduction of the Dallas Arts District fixture.

Private development on Flora Street has been guided by several provisions in the Sasaki Plan. All buildings located on Flora Street are required to step back from the street above the second story to ensure adequate light to the street. This will be continued in the current regulations, as



will requirements for transparency at the ground level, and the prohibition of driveways or service entry from the street.

Ann Williams Way, separated from Flora Street by One Arts Plaza, should be treated with the same standards as Flora Street, and as such, it is recommended that all guidelines for Flora Street be applied to Ann Williams Way wherever possible and advisable.

Above
Existing conditions on Flora St.

Opposite
Proposed character of Flora St., including prominent public art and active mixed-use building frontages





Priority Actions

- Replace unsafe and deteriorating sidewalks where needed.
- Discontinue use of Bald Cypress in new construction or replacement with appropriate street trees if existing trees are in poor condition.
- Improve existing lighting with fixtures that address street and pedestrian safety and energy efficiency.
- Establish new streetscape standards for retail uses.
- Incorporate sustainable features.

Implementation and Partners

Downtown Dallas, Inc.

- Coordinate with Dallas 360 thoroughfare recommendations to narrow pedestrian crossing at Pearl Street.

City of Dallas

- Approve and implement the Connect plan.

2017 Bond Issue & Future Funding Priorities

- Prioritize immediate funding for the Pearl Street/Flora Street intersection.
- Prioritize funding to the Betty B. Marcus Park to improve park visibility from Flora Street.
- Prioritize funding for sidewalk replacement at public facilities.

Dallas Arts District Actions

- Initiate public art program for temporary and permanent art installations in accordance with the City of Dallas public art cultural policy.
- Begin wayfinding initiative along Flora St./Ann Williams Way.

Opposite

2015 Chinese New Year celebration in the Arts District, hosted by the Crow Collection of Asian Art



Plan Framers.com

3

EMBRACE ROSS AVENUE AS A MIXED-USE COMMERCIAL CORRIDOR

Ross Avenue has long been considered as the edge of the Dallas Arts District. Even the Sasaki design guidelines stopped short of the south side of the street. While modest improvements have been made over the years, Ross Avenue should become one of Dallas' most successful urban mixed-use boulevards. Already several new mixed-use projects are emerging that will define the character of Ross Avenue as a destination. The City is investigating Ross as a potential transit corridor connecting Downtown with some of Dallas' most popular residential districts and historic neighborhoods such as Lower Greenville and Swiss Avenue.

The future of Ross Avenue should not be left to chance. Extending the Dallas Arts District to San Jacinto Street will ensure adequate design guidance on both sides of Ross Avenue and cross streets extending to San Jacinto. Strong

design guidelines that have served Flora Street over the years should be applied to Ross Avenue to ensure an equally high-quality streetscape and active street frontages free from numerous curb cuts or blank walls. Additional setbacks on Ross Avenue will be needed to support a transit-rich corridor for the future. The control of buildings entrances, service access, ground-level uses, and screening of parking will do more to establish the suitability of future development than prescribed land uses. A vibrant mix of land uses along the length of Ross Avenue should be a goal of the plan.

New pedestrian-scale lighting, paving, and street-tree standards on both sides of the street will establish continuity along Ross, but also as an extension of the Art District. Parcels along Ross Avenue will be subject to development standards to limit new driveways or service access.

Development guidelines will establish a required ground-level setback, with allowable private outdoor uses to avoid restricting the public walkways necessary for a walkable district. Additionally, maximum setbacks will ensure a more continuous street wall of active uses and ground-level transparency requirements will limit blank walls at street level. Parking structures facing Ross Avenue will need to be lined with active uses or substantially screened with architectural treatments.

The 2017 Downtown Dallas 360 Plan recommends increasing pedestrian and bike access on Ross Avenue by reducing the width of the existing vehicle lanes from 11' to 10' to allow for pedestrian and bike pathways on either side of the avenue. Additional pedestrian demands will require street-level amenities including trees, benches, and paving in coordination with the Connect plan.



Above and Left State St. in Chicago, a dense, mixed-use commercial corridor in a major American metro area, is an excellent example of the kind of character Ross Ave should strive to attain.

Images courtesy of Flickr user David Wilson



Above
Existing conditions along Ross Ave.

Opposite
A vision for Ross Ave, which includes a full mix of uses and a rebalancing of infrastructure that better accommodates pedestrians and transit.





Right
Families gather during the annual Dallas Arts District Block Party

Priority Actions

- Establish streetscape standards for private development along both sides of Ross Avenue within the Klyde Warren Park/Dallas Arts District Public Improvement District (KWP/DAD PID).
- Support the transformation of Ross Avenue into a safe, multimodal, and mixed-use urban corridor.

- Approve and implement the Connect plan.

Dallas Arts District Actions

- Initiate public art program for temporary and permanent art installations on Ross Avenue, in accordance with the City of Dallas public art cultural policy.

Implementation and Partners

Downtown Dallas, Inc.

- Coordinate with Dallas 360 thoroughfare recommendations to identify future transit needs on Ross Avenue.

City of Dallas

- Expand Dallas Arts District design guideline boundary to both sides of Ross Avenue.



4

EXPAND AND UPDATE SIGNAGE, WAYFINDING, PUBLIC ART, AND GATEWAY EXPERIENCES

Over the decades, a limited number of directional signs and monuments have been introduced to the District. Paired monumental pylons are located on the north side of Ross Avenue at Harwood and Olive Streets although they do not match each other. Various wayfinding signs are located on Pearl and on St. Paul Street as one approaches the District. It is arguable that such a system is unnecessary with today's nearly ubiquitous use of handheld and vehicle navigation devices. Many visitors will use these wayfinding tools (Google Maps, etc.) to arrive in the vicinity of their intended destination.

Those arriving on foot, or by hired ride or via transit will proceed to their address directly. If a visitor arrives by private vehicle, they will then need directions to find suitable parking locations that may not correspond to facility addresses. A consistent parking symbol, coordinated

with city standards, should be established to allow drivers to find public-parking locations. Unified parking signage would encourage visitors to park once and walk to various destinations within the district.

Despite the reduced need for exact directional signage facilitated by ubiquitous web-based mapping services on handheld devices, district identity should be strengthened by introducing public art at an urban scale to announce the Arts District to visitors at key gateways. Such a gateway-arts program should be located to embrace a larger area to extend beyond Flora Street to the Pearl Street and Ross Avenue corridors, the Woodall Rodgers service roads, and Routh Street as major gateways to the Dallas Arts District.

A total of four gateway corridors have been identified for installation of major public artworks as permanent or temporary installations.

Artwork should anchor the corridors. The exact location of art installations would be subject to space availability and suitability for the art installations selected. Arts installations would be coordinated with the street improvements covered in other strategies to enhance public movement and improved access to the art. The Dallas Arts District would establish a program and manage and find funding for the selection and installation of these gateway elements, as well as art along Pearl Street, in cooperation with the City and TXDOT.

For pedestrian visitors to the district, providing free WiFi and pedestrian-oriented information kiosks in the Arts District would empower and inform visitors. Up-to-date information on events and attractions in the district and downtown would enhance visitor experience and support co-marketing between institutions, retail, and restaurants. The Dallas Arts District has readily available content for such kiosks, and the Dallas Innovation Alliance is implementing a similar effort in the West End. Digital kiosk planning and implementation in the Arts District will recognize city-wide standards for such kiosks to avoid competing standards



Above

Top left: A digital kiosk guides pedestrians in Brooklyn, New York

Photo courtesy of Flickr user metafus.com

Top right: Digital kiosks in Seattle, Washington provide information for transit riders

Photo courtesy of Flickr user SDOT Photos

Bottom left: New Orleans wayfinding signs orient pedestrians and enhance neighborhood character.

Photo courtesy of Flickr user Eric Fischer

Bottom right: Wayfinding signs in Downtown Boston guide visitors and locals and define the neighborhood.

for information display, installation, and maintenance. Furthermore, more subtle, static ground-based signage can signal to pedestrians that they have crossed into the Arts District. These low-cost and low-maintenance wayfinding options can help visitors confirm that they are in the right place, while also helping establish a consistent brand in the Dallas Arts District.

Within the District, it is intended that various permanent or temporary art installations will be curated and installed within the public open spaces and sidewalks. Placement of public art in the public right-of-way is encouraged as a means of adding variety to the streetscape and enabling free and public interaction with the arts. Selection of appropriate artworks within the public realm will be the primary responsibility of the Dallas Arts District Public Art Committee, in alignment with the City's Public Art Committee. Artwork will need to conform to requirements established by the City of Dallas and the Connect plan to avoid blocking safe passage of pedestrians or distracting drivers. However, as site-specific art

installations will vary, it is not advised that specific requirements be established that may constrain future art concepts from realization.

Building-signage issues must also be addressed through future legislation or regulation. Evidence suggests that commercial and retail tenants have turned away from the Arts District in the past due to signage requirements that are far more stringent than the rest of Downtown Dallas. Though the Arts District is a unique environment, it should encourage a diverse range of uses that include small business, corporate, and retail tenancy; therefore, its signage requirements should not discourage such uses or push them to other parts of Downtown.

Signage and wayfinding efforts should be focused on key entry points to the District. Four such gateways have been identified as major routes into the Arts District, based on pedestrian, vehicle, and transit flows.

Opposite page

Conceptual illustration of Pearl St., looking north toward Ross Ave., showing the use of public art as a gateway feature.



Priority Actions

- Adopt new or amend existing signage regulations specific to the Arts District that encourage a cohesive experience while encouraging a wide range of uses and tenants and recognizing the operational needs of District members.
- Locate new gateway art installations for larger District identity.
- Install ground-based wayfinding at key pedestrian and non-motorized locations.
- Improve WiFi access for visitors with smart kiosks modeled by Dallas Innovation Alliance.
- Improve distribution of information for visitors through kiosks and WiFi.
- Assist visitors in finding public parking with standard signage.

Opposite

Strategy 4 provides a roadmap to transforming key routes into and through the Arts District.

Implementation and Partners

City of Dallas

- Coordinate with city-wide parking signage standards.
- Approve and implement the Connect plan.
- Coordinate smart kiosks with Dallas Innovation Alliance.

Dallas Arts District Actions

- Initiate public art program for temporary and permanent art installations at gateways.
- Research and install effective wayfinding along pedestrian routes.
- Initiate tactical short-term wayfinding at the Pearl/Arts District DART Station to identify clear routes to Dallas Arts District destinations from transit and parking hubs.



5

ENHANCE PEDESTRIAN CONNECTIONS

Focusing on the Perot Museum, West End, & Victory Park

Most of the streets that cross through the Dallas Arts District have been designed and constructed to prioritize automobile flows. The wide vehicle lanes, limited sidewalk plantings, high vehicle-volume signalization, and poorly defined street edges are all hallmarks of an earlier period in urban life when cars were far more dominant than the best urban thinking believes they ought to be.

This standard has created an environment in which pedestrians feel neither safe nor welcome, a condition completely antithetical to the vision for the Arts District's future. As a place that will encourage a broad mix of uses and users, all travelers, regardless of mode, must feel comfortable entering and traversing the Arts District. By rebalancing the focus toward pedestrians, streets in the Arts District will create a more welcoming and inclusive environment.

All of the street typologies established in this document address the need for improved pedestrian connections. Sidewalk, Traffic Calming, and Development Control requirements and recommendations have all been developed with comfortable foot travel as a key priority.

Perhaps one of the greatest challenge for the Arts District is the tenuous pedestrian connection from the Klyde Warren Park to the Perot Museum of Nature and Science. While many streets in the district could and should be enhanced for pedestrians, the Woodall Rodgers Freeway and its many high-speed on- and off-ramps create a physically intimidating barrier. However, enhancing pedestrian connections between the Arts District, the West End and Victory Park; existing transit (bus, light rail, and D-Link); and future D2 DART and streetcar routes is of primary importance to the Arts District and the City of Dallas.

While long-range plans should contemplate the eventual diminishment of highway impacts on the City (as Klyde Warren Park has so well demonstrated), it is unlikely that the freeway or its many disruptive ramps will be significantly adjusted in the short term. For this reason, it is critical to improve the pedestrian realm to overcome the dominance of high-speed auto traffic. This can only be achieved with sidewalks and crossings that protect vulnerable pedestrians while slowing traffic to more urban speeds.

North Field Street also presents a barrier between the Arts District, the West End and Victory Park. The discontinuity created by this District Connector is palpable to any pedestrian who attempts to cross the eight lanes of traffic. Given the significant attraction of Klyde Warren Park and the Perot Museum, this obstacle deserves special attention to improve pedestrian crossing conditions. While pedestrian bridges have been proposed, improved at-grade connections will do more, in the long term, to calm traffic and enhance the pedestrian experience. A variety of methods including pedestrian-refuge areas, priority signal timing, bold street markings or traffic tables should be

implemented to reduce excessive traffic speeds and enhance pedestrian safety.

Between North Field and St. Paul Streets, the public right-of-way along North and South Woodall Rodgers frontage roads should be enhanced to protect pedestrians from high-speed traffic. On blocks without additional public rights-of-way, the city should negotiate with private developers to expand the public realm through land swaps or other incentives. At locations where the City does have rights-of-way, improvements should be prioritized to enhance pedestrian safety. The City should also negotiate with TXDOT to eliminate unnecessary Texas U-turns at North Akard and Pearl Streets and replace these areas with enhanced pedestrian areas. North and South Woodall Rodgers frontage roads should be renamed to help transform public perception of these streets from high-traffic highway ramps to accessible local roads in keeping with the character of the Arts District. Renaming these streets would help convey their role as key connection points and destinations within the district.



Above
Existing sidewalk conditions at the corner of N. Field St. and Woodall Rodgers frontage road, westbound.

Opposite
View of the same intersection with proposed ground-level pedestrian infrastructure improvements.



Priority Actions

- With the help of key stakeholders, establish safe pedestrian connections between the Arts District and other amenities and districts.
- Support connectivity needs of the Field/Griffin corridor.
- Coordinate with DDI, City planning entities, and TXDOT to facilitate infrastructure improvements.

Implementation and Partners

Downtown Dallas, Inc.

- Coordinate with Dallas 360 thoroughfare recommendations to improve pedestrian crossings at N Field and Woodall Rodgers access roads.

City of Dallas

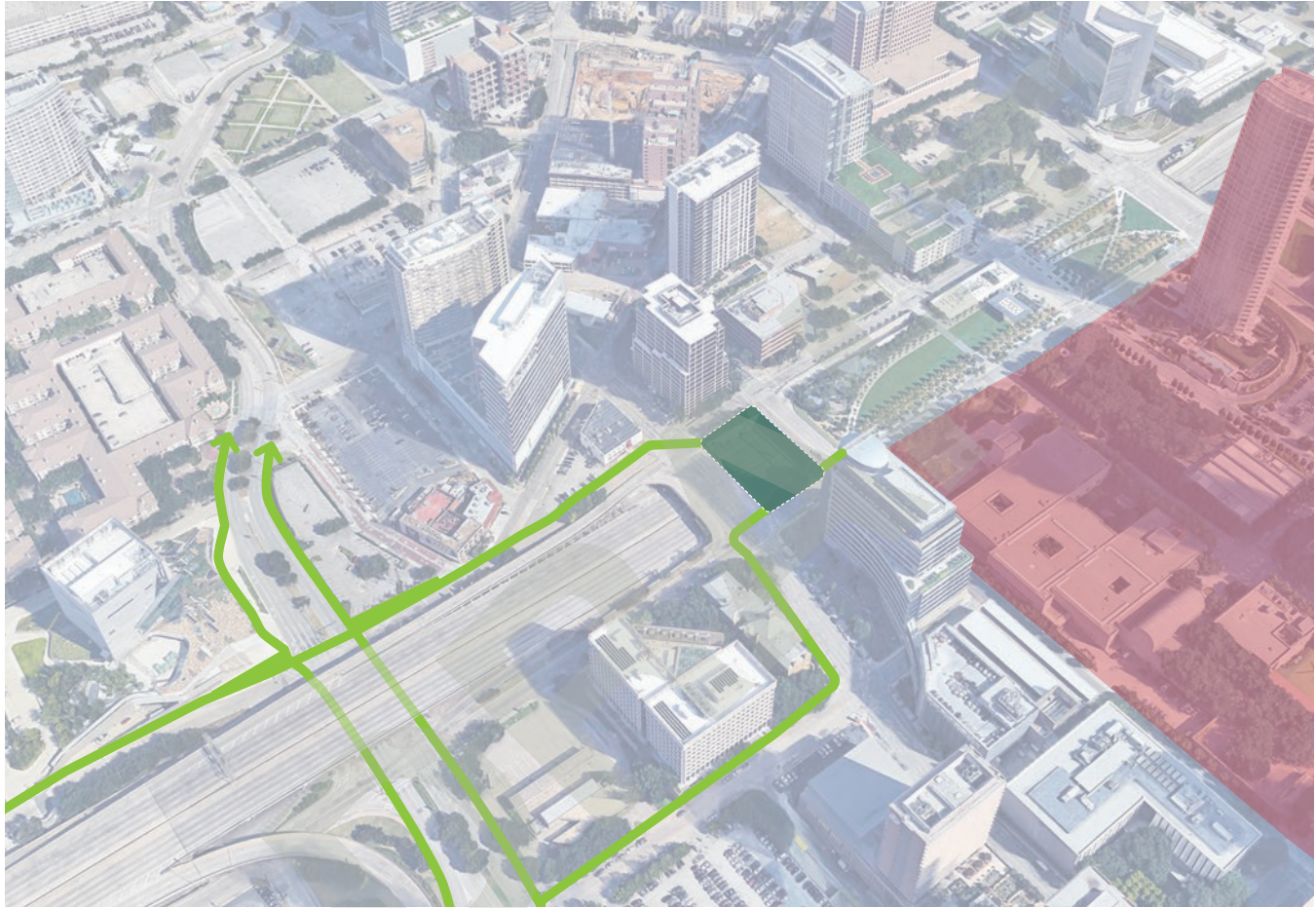
- Approve and implement the Connect plan.
- Rename Woodall Rodgers Freeway North and Woodall Rodgers Freeway South (service/access roads) to reflect a more urban address.

Texas Department of Transportation (TxDOT)

- Advocate for removal of Texas U-Turn at N. Akard Street and Pearl Street.
- Prioritize Funding for sidewalk improvements on publicly owned ROW within corridor.

Dallas Arts District Actions

- Initiate public art program for temporary and permanent art installations between Perot and Klyde Warren Park.
- Research and install effective wayfinding along pedestrian routes.



Above
A series of greener, at-grade pedestrian connections between the Arts District (red), Klyde Warren Park, the West End, Victory Park, and the Perot Museum.

DESIGN GUIDELINES

DRAFT

PURPOSE

The Sasaki Plan has served the Dallas Arts District well for over 30 years. As the Arts District has developed, several components of the Sasaki Plan need to be revised to better support active street-level uses, ADA compliance, and an evolving understanding of sustainability. Many aspects of the Sasaki Plan have proven problematic for compliance by private developers, while fine arts institutions have, in general, ignored the requirements. Furthermore, the process of approvals as eventually adopted by the City, has proven to be inflexible and time-consuming when exceptions to the guidelines are requested. Thus, the Sasaki Plan Design/Development Guidelines and the process for approvals will be addressed in the Dallas Arts District Connect Master Plan and relevant zoning.

The purpose of the Design Guidelines section is to:

- Create a cohesive District identity and character;
- Prevent design decisions that result in negative social and environmental impacts;
- Enable coordination between private developers and the City;
- Coordinate all planning efforts with Downtown Dallas, Inc.; and
- Assist reviewers in making project recommendations by providing concrete principles against which a project can be judged.

The Design Guidelines should be used by designers to create a project that embodies the 5 Key Strategies, in support of the Vision for the Dallas Arts District contained in this document.

Relevant zoning establishes the basic requirements for a proposed project, and these guidelines help establish the best means by which proponents can meet those requirements, as recommended by a committee of design and development professionals who consider the best interests of both the Arts District and the overall City.



Left
Flora Street during
the Dallas Arts District
Block Party in 2017

STREET TYPOLOGIES

The design guidelines will apply to areas within the boundary of the Dallas Arts District. However, the guidelines extend to areas outside the current boundaries of the District to provide additional guidance for projects subject to discretionary review by the City for any reason. Should the boundaries of the District be extended in the future, these guidelines would be relevant and apply to those areas within the Arts District boundary.

The Guidelines have been organized by street typologies. While some streets, such as Pearl, Flora, and Ross, will have specific guidelines related to those streets alone, other streets have been classified into categories for ease of application. If any new streets or previously abandoned streets are platted, the City of Dallas will work with the Dallas Art District and other area stakeholders to determine the appropriate typology for such streets.

The 6 street typologies are:

- 1) **Pearl Street: “Avenue to the Arts”**
- 2) **Flora Street/Ann Williams Way: Mixed-Use Cultural Corridor**
- 3) **Ross Avenue: Mixed-use Commercial Corridor**
- 4) **Pedestrian Priority Streets**
- 5) **District Connectors**
- 6) **Service-Oriented Streets**

Opposite page

A map of all street typologies that are relevant to the Connect plan. All illustrated typologies fall within the existing KWP/DAD PID boundary but go beyond the existing DAD boundary; this is in recognition of the need for strong interconnections between the District and its surrounding neighborhoods.

DISTRICT-WIDE DESIGN GUIDELINES:

There are principles and design standards that are intended to be applied throughout the Dallas Arts District Connect Master Plan area. Regardless of street typology or specific site condition, these standards are based on best practices for landscape and urban design and should be the starting point for any proposal occurring in the Connect plan area. Their aim is to improve human safety and comfort, environmental sustainability and resilience, and design quality while maintaining a cohesive character for the Dallas Arts District. Whenever possible and feasible, projects should consult the Texas Trees Foundation's Southwest Medical District (SWMD) Urban Streetscape Master Plan report for baseline guidance on planting, paving, and lighting performance standards. All projects should employ sustainable design practices appropriate for the climate of Dallas, and when possible and feasible should consider sustainable building and design standards established by recognized rating and certification systems.

Right

Movable street furniture and sidewalk plantings in Grand Park in the Downtown Los Angeles Arts District.

Photo courtesy of Flickr user Hosie Thomas

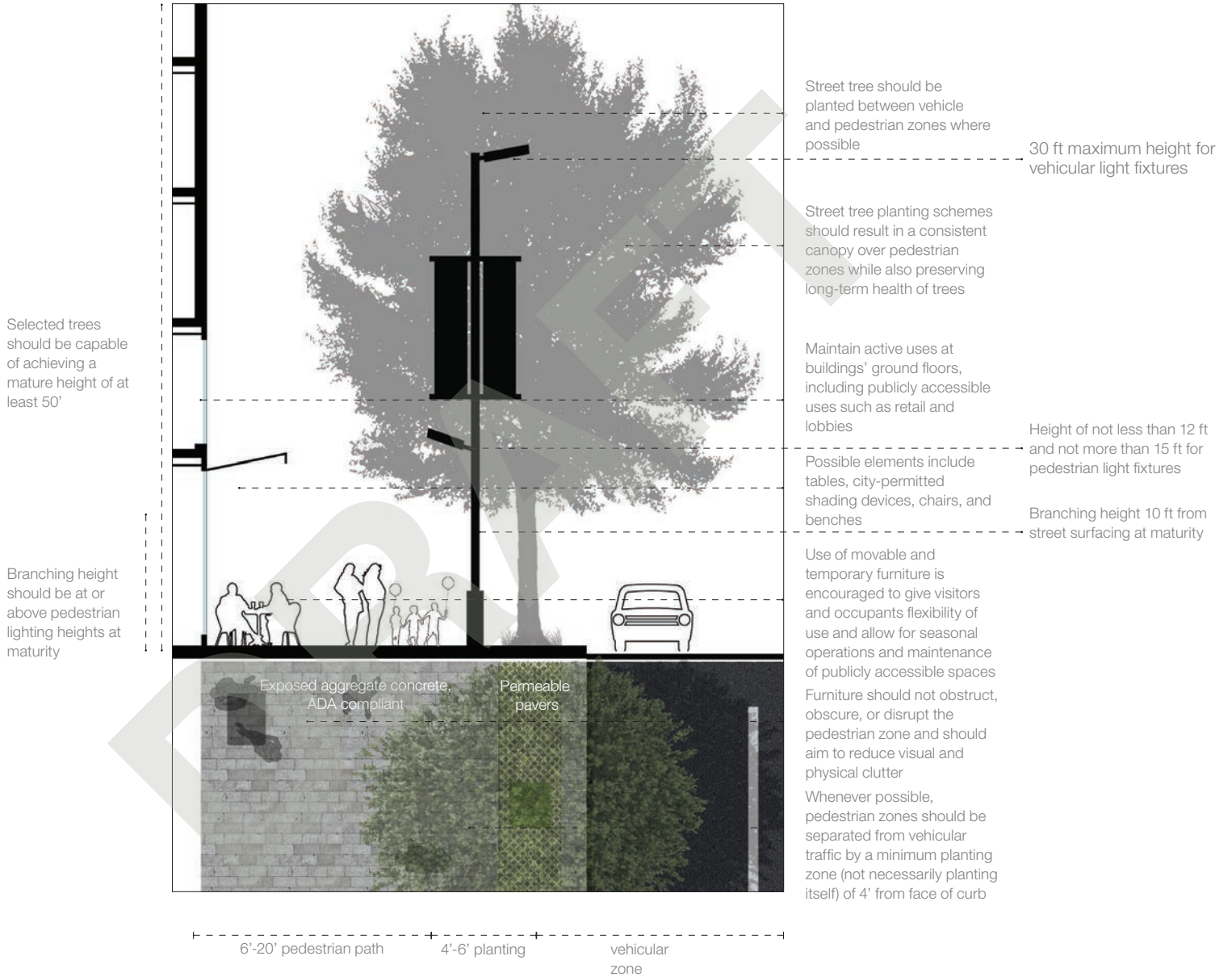
Curb extensions shorten crossing time, slow traffic, and protect pedestrians while providing area for street planting and stormwater management interventions like bioswales
Photo courtesy of the Flickr user NACTO



1. SIDEWALKS

Landscape Systems

- a) Use low-impact development and environmentally responsible design strategies to reduce energy use and the impacts of the urban heat-island effect.
 - i) Wherever possible, streetscape corridors are to utilize green strategies to maximize energy savings and improve the Dallas Arts District's heat island.
 - ii) Pavements that store less heat and have lower surface temperature are recommended over conventional materials. Light-colored surface treatments help heat absorption and, therefore, surface temperatures. Streetscapes should employ strategies to maximize energy savings and mitigate urban heat-island effects.
 - iii) Specify products that have light colors, thereby increasing the reflectivity of the finished pavement.
- b) Improve Outdoor Air Quality by implementing strategies that mitigate and prevent the excess buildup of Carbon Dioxide (CO₂) in the air. Where possible, utilize recycled and reclaimed paving types. The amount of CO₂ that trees can absorb makes them an effective mitigation tool.
 - i) Prioritize trees with longer life span to reduce CO₂ content in the atmosphere and avoid the need for replanting.
 - ii) Reuse salvage materials and locally sourced materials when possible.
 - iii) Groundcovers and native meadow grasses should be prioritized over lawn whenever possible.
 - iv) Utilize materials with life cycles longer than 10 years whenever possible.
 - v) Prioritize standard material sizes to reduce overall project waste whenever possible.
- c) Effectively manage stormwater through proper planting and surface design. Vegetation and soils can reduce the amount of stormwater runoff and mitigate adverse impact to the site's water resources. The vegetation intercepts rainfall, and the exposed soils associated with the plants absorb the water that will return to groundwater systems or be utilized by the plants.
 - i) Permeable pavers should be used where appropriate in order to sufficiently support stormwater infiltration and management, particularly in non-pedestrian zones.



- ii) Consider the appropriateness of rain gardens and other integrated stormwater-management approaches. Select plant species that capture pollutants for infiltration basins.
- d) Street trees and other vegetation should support placemaking, screening of utilities, human comfort and inspiration, stormwater management, urban heat-island-effect mitigation, and enhancement of the District's programming.
- i) Plantings should not obstruct site lines to traffic or visibility of intersections or signs near roadways. Reference the City of Dallas sightlines criteria for further guidance.
 - ii) Multilayered planting strategies should be implemented in open beds; utilizing groundcovers, shrubs, ornamental grasses, perennials, and canopy trees.
 - iii) Encourage diversified planting strategies to add aesthetic value and improve planting performance and long-term health. Avoid monoculture planting to ensure tree presence if a particular tree species fails. Use the relevant recommended adapted and native planting reference guide for planting selections. Consult the recently published *Southwestern Medical District Urban Streetscape Master Plan: A Prescription for Dallas, Texas* (November 2016, page 49) for suggested species.
 - 1) The maximum limit of annual plantings should be 5% of the total planting design. No more than 50% of street trees on any block should be of one species, and no more than 33% of ornamental trees in any project should be of one species.
 - iv) Maintain a visually simple and clear spatial arrangement through plant massing with single plant species clustered in no fewer than 12 plants.
 - v) The ground plane surrounding a street tree may be planting bed, stormwater infiltration basin, or pavement over Silva Cell (or equal) substrate. Planting bed surface can consist of groundcover, hedges, or ornamental grasses not exceeding 24" in height. In paved conditions, tree trunk pavement openings should be a minimum of 24" diameter. Metal tree grates are discouraged due to difficulty of maintenance and possibility of "choking" trees.
 - vi) Maintain existing healthy trees that have 10" caliper or greater when possible.
 - vii) Street trees should be planted between vehicle and pedestrian zones where possible.
 - viii) Street tree planting schemes should result in a consistent canopy over pedestrian zones while also preserving long term health of trees. Where two rows of trees are

planted, the rows may be staggered or aligned with each other.

- ix)** Wherever possible, use non-potable water sources and consider the use of integrated stormwater collection for irrigation. Two tree bubblers per tree are recommended.
 - x)** Plantings should be appropriate for climate and design intent; considerations include cold hardiness, disease resistance, well-shaped canopies with consistency, heat tolerance, soil volume requirements, and maintenance requirements. Any planted tree should be well-suited to the North Texas climate and have demonstrated success as a street tree. Bald cypress should be avoided due to their demonstrated unsuitability as street trees within the Arts District.
 - xi)** Selected trees should be capable of achieving a mature height of at least 50'.
 - xii)** Street trees should be capable of achieving a mature canopy of at least 40' in diameter. Branching height must be a minimum of 10' from street surface at maturity. To ensure branches do not obscure pedestrian lighting, branching height should be at or above pedestrian lighting heights at maturity.
- e)** Soil engineering should consider overall vegetation health as well as the effects on stormwater management and irrigation cost and effectiveness.
- i)** Soil densities should be high enough to avoid settling, yet low enough to allow adequate root growth.
 - ii)** Soil profiles should be engineered to meet the needs of the selected vegetation. Soil samples should also be obtained in order to amend the soil per the plants' growing needs.
 - iii)** Street trees should be planted in at least 600 cubic ft of engineered topsoil, with 850 cubic ft preferred; ornamental trees at least 350 cubic ft, with 450 cubic ft preferred; and small ornamental trees at least 200 cubic ft, with 250 cubic ft preferred.
 - iv)** A soil depth of at least 5 ft is recommended for street tree plantings.
 - v)** Ornamental trees, groundcover, and other plantings should have appropriate soil depths for species and site conditions.

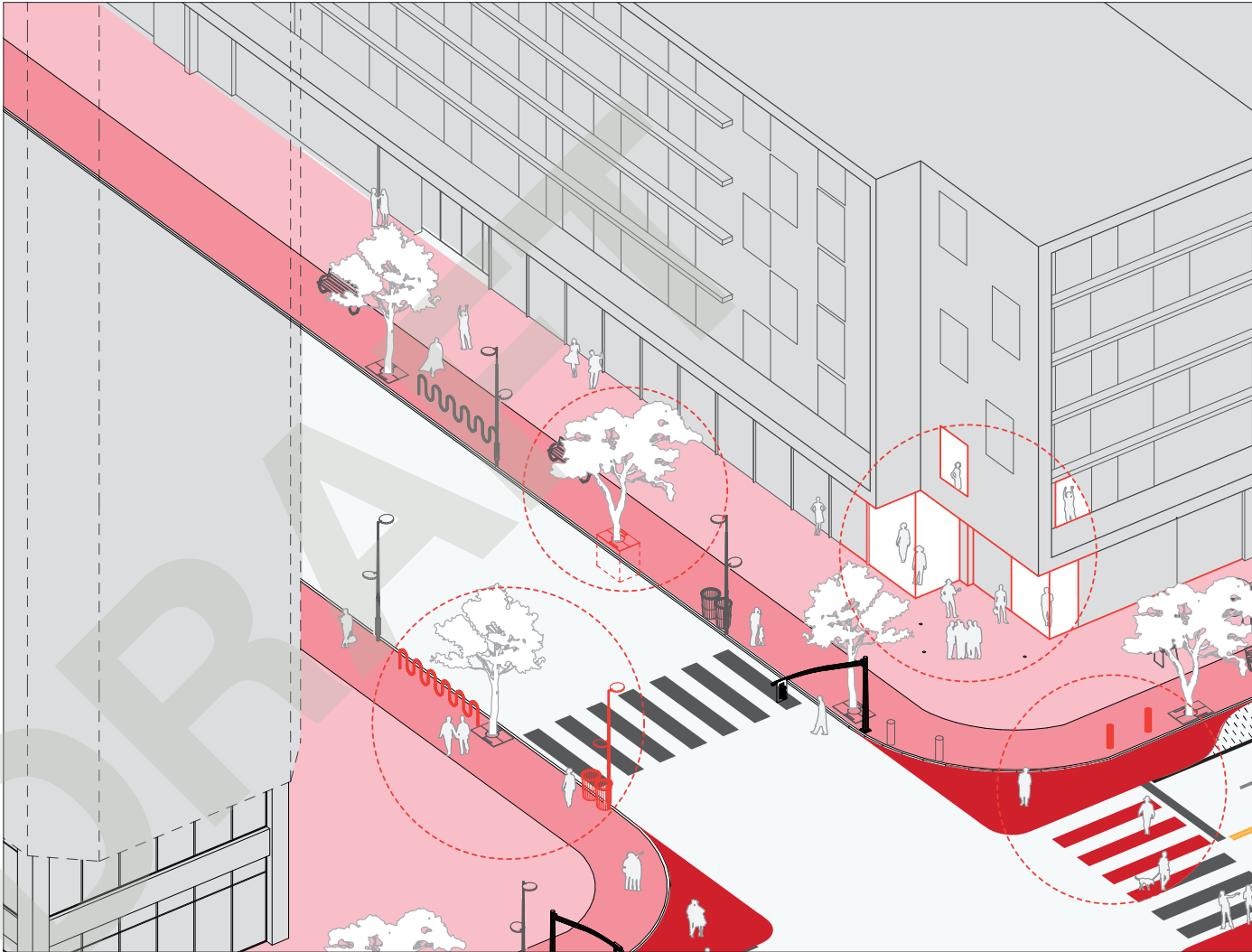
Pedestrian Zones & Site Elements

- a)** In order to improve the walkability of the space, clear, connected, and safe pedestrian

circulation routes should be a top consideration for every project.

- i)** All pedestrian zones should remain clear, connected, and safe.
 - ii)** All joint types shall always be designed and located aesthetically as necessary.
 - iii)** When stairs are needed, utilize steps with low risers and wide treads in order to promote ease of movement where appropriate.
 - iv)** Areas for seating shall occur along streetscape, but outside of the designated pedestrian walkway to keep circulation path clear.
 - v)** Wherever possible, pedestrian pathways should be separated from vehicular traffic by a minimum planting area of 4 ft from face of curb (though not necessarily planting itself).
 - vi)** Universal design standards are encouraged for the equal safety and enjoyment of site features and amenities by all users.
- b)** Paving materials should be selected to enhance the consistent aesthetic character of the Arts District while diminishing the need for short- and long-term maintenance.
 - i)** Unit pavers should be used to highlight pedestrian areas associated with dwelling spaces, entries, plazas, parks, and courtyards.
 - ii)** Salvage and reuse existing site materials when possible.
 - iii)** Materials with recycled content are strongly encouraged.
 - iv)** Prioritize the use of standard material sizes to reduce overall project waste and improve ease of maintenance.
 - v)** Utilize materials with life cycle over 10 years.
- c)** Street furnishing and elements should support accessibility, informal social interactions, cohesive aesthetics, District cleanliness, and bicycle usage.
 - i)** Possible furnishings and elements include tables, umbrellas, chairs and benches, bike racks, bike boxes, bollards, fencing, waste receptacles, signage, ash urns, free-standing planters, information kiosks, bus shelters, and shade structures. Media stands are strongly discouraged along streetscape.
 - ii)** Roofed shelters are encouraged for all transit stops (bus, light rail, street car) within the District and should meet or exceed City of Dallas standards. Transit shelters should not disrupt continuous pedestrian zones.

Selected trees should be capable of achieving a mature height of at least 50'



50% of storefronts to be transparent on first two floors

Bubblers, 2 per tree

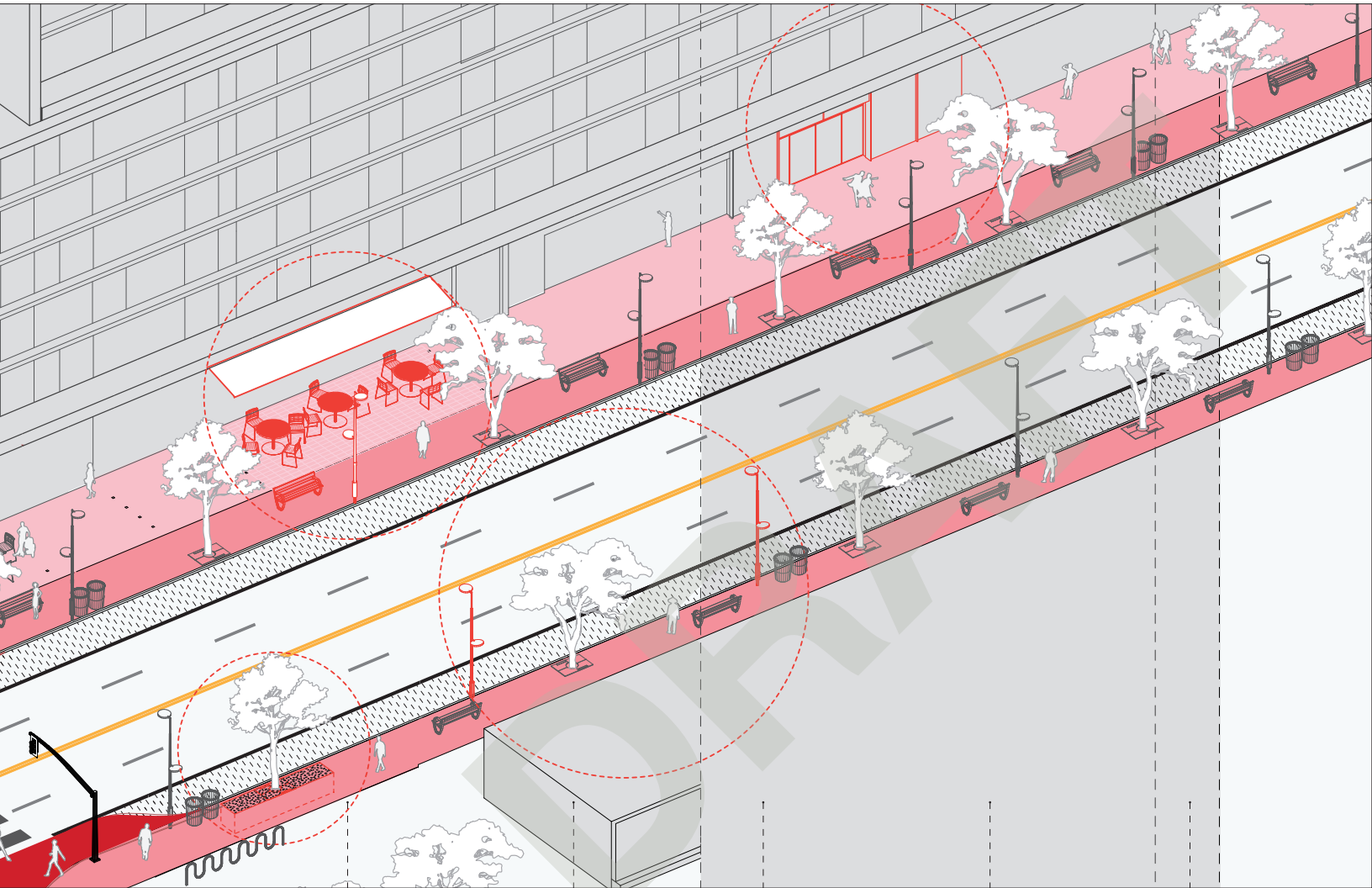
Allowable public furniture - chairs, tables, umbrellas, and benches

Allowable site elements - bike racks, bollards, fencing, waste receptacles, signage, planters, and information kiosks

A soil depth of at least 5 ft is recommended for street-tree plantings

At least 50% transparency for the first two aboveground floors, with an emphasis on transparency and active uses at street corners

Street trees should be capable of achieving a mature canopy of at least 40' in diameter



Adopt multilayer planting system

Waste receptacles near street traffic, intersections, or high traffic areas

Maximum of 33% ornamental tree of one species

Hidden drip-irrigation system

Maximum of 50% shade tree of one species

Street-tree spacing shall be no less than 25 ft on center and no more than 40 ft on center.

- iii) Bike racks should be located near amenity zones such as intersections, entrances to buildings, transit stops and stations, and open spaces.
 - iv) Trash and recycling receptacles should be located in amenity zones near street crossings and intersections or in other high-traffic areas.
 - v) Provide a family of site element types that can be adapted throughout the Dallas Arts District to establish a unified streetscape aesthetic.
 - vi) Highly durable, resilient, and sustainable materials will help avoid the need for replacement and excessive maintenance.
 - vii) Use of movable and temporary furniture is encouraged, in order to give visitors and occupants flexibility of use and allow for seasonal operations and maintenance of publicly accessible spaces.
- d) Lighting should enhance the perceived safety and visual interest of the District and create a safe nighttime environment for pedestrians, bicyclists, and vehicles.
 - i) Use high energy-efficiency-lighting fixtures whenever possible. Mercury vapor, low-pressure sodium, high-pressure sodium, and metal halide light sources are strongly discouraged.
 - ii) Building lighting should not cast light on adjacent property.
 - iii) Street- and pedestrian-lighting poles should be equally spaced between trees along streetscape corridors when possible.
 - iv) Adapting the existing white Dallas Arts District lighting fixture to accommodate high efficiency light sources and appropriately designed pedestrian lighting is encouraged, to maintain consistency of character.
 - v) Consistent color temperature within and across property boundaries is strongly encouraged to support cohesive experience throughout the District.
- e) Buffer elements such as walls, fencing, and miscellaneous screening methods should enclose and define space, provide scale and interest, and enhance the pedestrian and bicyclist experience and streetscape character.
 - i) Service areas, building systems, and other unfriendly uses should be screened from adjacent land uses and the public realm by vegetation or architectural features.

- ii) Walls and fences adjacent to outdoor public spaces should be made of high-quality materials such as stone, metal, concrete, or other opaque materials.
 - iii) Chain-link, vinyl, and plastic fences are strongly and explicitly discouraged due to the low design quality such buffers represent.
 - iv) Walls and fences that complement the design character of adjacent buildings and appear as extensions of the building walls or bases are encouraged when they are advisable for screening purposes.
 - v) Excessive or superfluous screening is strongly discouraged in all instances.
- f) Street furniture and site elements should support pedestrian comfort and enhance access to the Arts District. Seating is encouraged on every block to provide respite and encourage pedestrian use of the District.
- i) Public street furniture should be of high-quality and durable materials. Consistency of material and frequency are recommended within specific projects.
 - ii) Site elements should be of high quality and durable materials. Possible elements to be considered include bike racks, bollards, fencing, waste receptacles, signage, free-standing planters, and information-delivery infrastructure.
 - iii) Bike lanes, bike racks, bike boxes, and other forms of bicycle infrastructure are strongly encouraged and should be installed where appropriate and in sufficient amounts to accommodate at least 10% of average daily trips to a site or city-wide commute mode splits, whichever is higher.
 - iv) Areas for seating should occur along streetscape, but outside of the designated pedestrian zone to keep circulation paths clear. Furniture and site elements should not obstruct, obscure, or disrupt the pedestrian zone and should aim to reduce visual and physical clutter.

2. DEVELOPMENT CONTROLS

Site Planning

- a) All curb cuts, whether new or reconfigured, should be perpendicular to pedestrian zones and existing rights-of-way so as to reduce curb-cut width and improve visibility for vehicles. Angled curb cuts are strongly discouraged. At least 12 ft of pedestrian zone should separate any two curb cuts. When reconfigured, curb cuts should be limited to 28 ft in width if more than one lane and 14 ft if a single lane, in order to reduce interruptions to façade and

pedestrian zone, and support an active street edge. Curb cuts should be minimized to reduce the negative impacts of a discontinuous pedestrian environment punctuated by vehicle activities.

- b) Loading should occur on Service-Oriented Streets whenever possible to reduce the impacts of building operations on streets with which passersby are most likely to interface. Existing loading docks may remain and/or be rebuilt.
- c) Avoid the creation of superblocks with public-realm access points spaced more than 600 ft apart. Use streets or vehicle-excluded ways to provide pedestrian access across blocks at short, regular intervals.
- d) Avoid visibility of utilities from the public realm wherever possible.

Site Uses

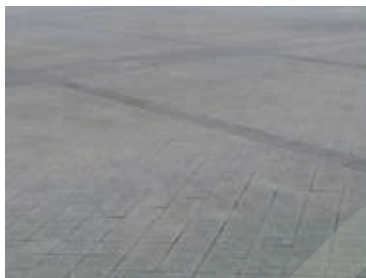
- a) Maintain active uses at buildings' ground floors, including publicly accessible uses such as retail, lobbies, and cultural institutions.
- b) Ground-level parking should be avoided, even when screened.
- c) Corner lots in particular should focus on active uses, because they are locations where pedestrian volumes will be highest.
- d) Orient primary building-entry points to the public realm in order to provide clear access and communicate the importance of the public realm. Entrances should be clearly visible from the public realm and well marked.
- e) Publicly accessible, active ground-floor uses should be available on every block that is not Service-Oriented, and placed as frequently as possible to support a diversity of experiences. Service-Oriented streets can also have such uses, but it is not a top priority.

Site Operations

- a) Passenger operations should occur within existing vehicular right-of-way abutting the sidewalk and pedestrian zone. Curb bump-outs or extended curbs are recommended to protect passengers accessing pick-up/drop-off zones, and enlarging of vehicular zones is strongly discouraged. Roadway extensions into existing pedestrian zones or private property to accommodate pick-up/drop-off operations are strongly discouraged.
- b) Accommodations for non-motorized transportation should be provided alongside all existing and proposed uses, including sufficient bicycle storage and tenant-accessible showers.
- c) Ground-level parking and vehicle turnarounds should be avoided.

Opposite

Examples of light posts and successful existing pavement schemes that support the desired character of the Arts District. White posts that are respectful updates to the existing posts with slimmer profiles and lower pedestrian-oriented fixtures are ideal. Larger granite block pavers on a stable substrate or other material of consistent color and texture are encouraged for paving.



- d) Avoid placing building mechanical systems along street frontage, even when screened, whenever possible in order to reduce negative noise and visual impacts on pedestrians.

Building Form

- a) Limit building setbacks in order to maintain continuous street edges and active streetscapes. Exceptions may include well-defined courtyards that do not significantly disrupt a continuous street edge.
- b) Maximize façade transparency for the first two floors of buildings to provide an active streetscape. Effective transparency maintains continuous visual interest for pedestrians and allows regular visibility into building interiors. Long, horizontal stretches of blank or opaque surface at any height are strongly discouraged.
- c) Orient primary building-entry points to the public realm to provide clear access and communicate the importance of the public realm.
- d) Any crossing of the public realm that is not at-grade, where not explicitly prohibited, is strongly discouraged.
- e) All proposed buildings and landscapes should adhere to LEED-ND principles for Green Infrastructure and Buildings. Proposed projects should also give special consideration to principles described in LEED-ND sections on Site Location and Linkages and Neighborhood Pattern and Design.
- f) Adverse visual, auditory, and other environmental impacts should be analyzed and minimized in an effort to create a harmonious District with structures and entities that support the District's goals. Building reflectivity and its impacts should be analyzed during schematic-design and design-development phases. Design solutions to minimize potential off-property impacts, visual impairment, and ecological damage should be considered.
- g) Advocate for signage regulations that are suitable within the downtown context and complement downtown signage, while establishing a distinct identity and character in the Arts District.

3. PUBLIC ART

- a) All publicly displayed art placed in the Arts District should support and promote the vision for the Arts District provided in this plan.
- b) The Arts District is expected to determine the criteria by which proposed public art installa-

tions will be judged and approved within the Arts District.

- c) Public art placement should not interfere with visibility, existing utilities or access to utilities, street and pedestrian lighting, landscape areas, regulatory signage, vehicular and pedestrian circulation patterns, or minimum pedestrian zone widths.

4. TRAFFIC CALMING

Vehicle Lanes

- a) Lanes should be no more than 11.5' (except Flora Street) to reduce travel speeds and create a safer environment for non-motorized travel.
- b) Advocate for narrower lane widths with City and State transportation agencies whenever possible.

Vehicle Speed

- a) Advocate for reduced, posted vehicle-travel speeds of 25 miles per hour wherever possible.

Pedestrian Safety

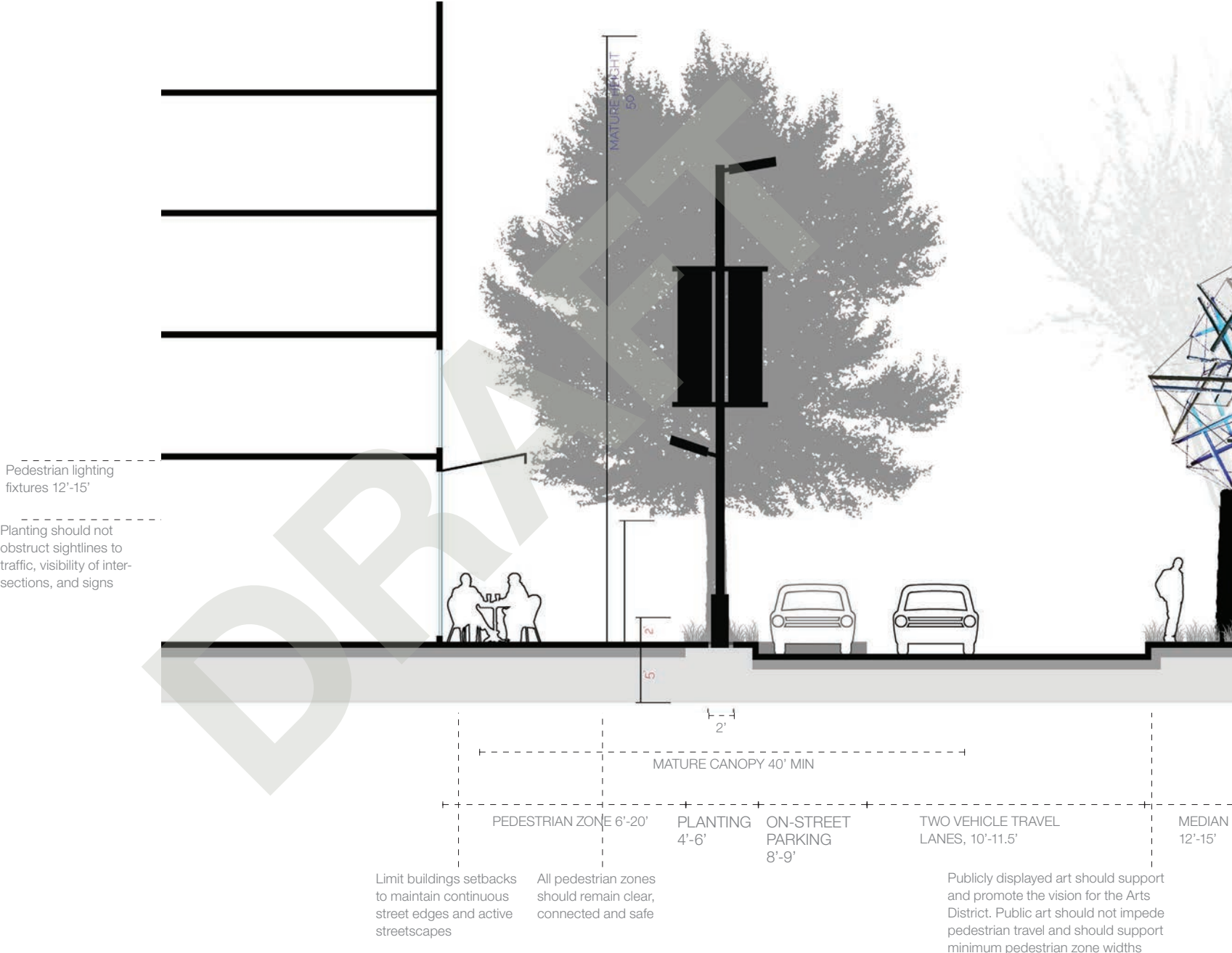
- a) Install curb bump outs wherever possible on roads wider than two travel lanes to minimize pedestrian-crossing distances and increase pedestrian safety.
- b) Support and adopt infrastructure to encourage bicycle travel, including bicycle corrals, bicycle lanes, sharrows, and dedicated signals.

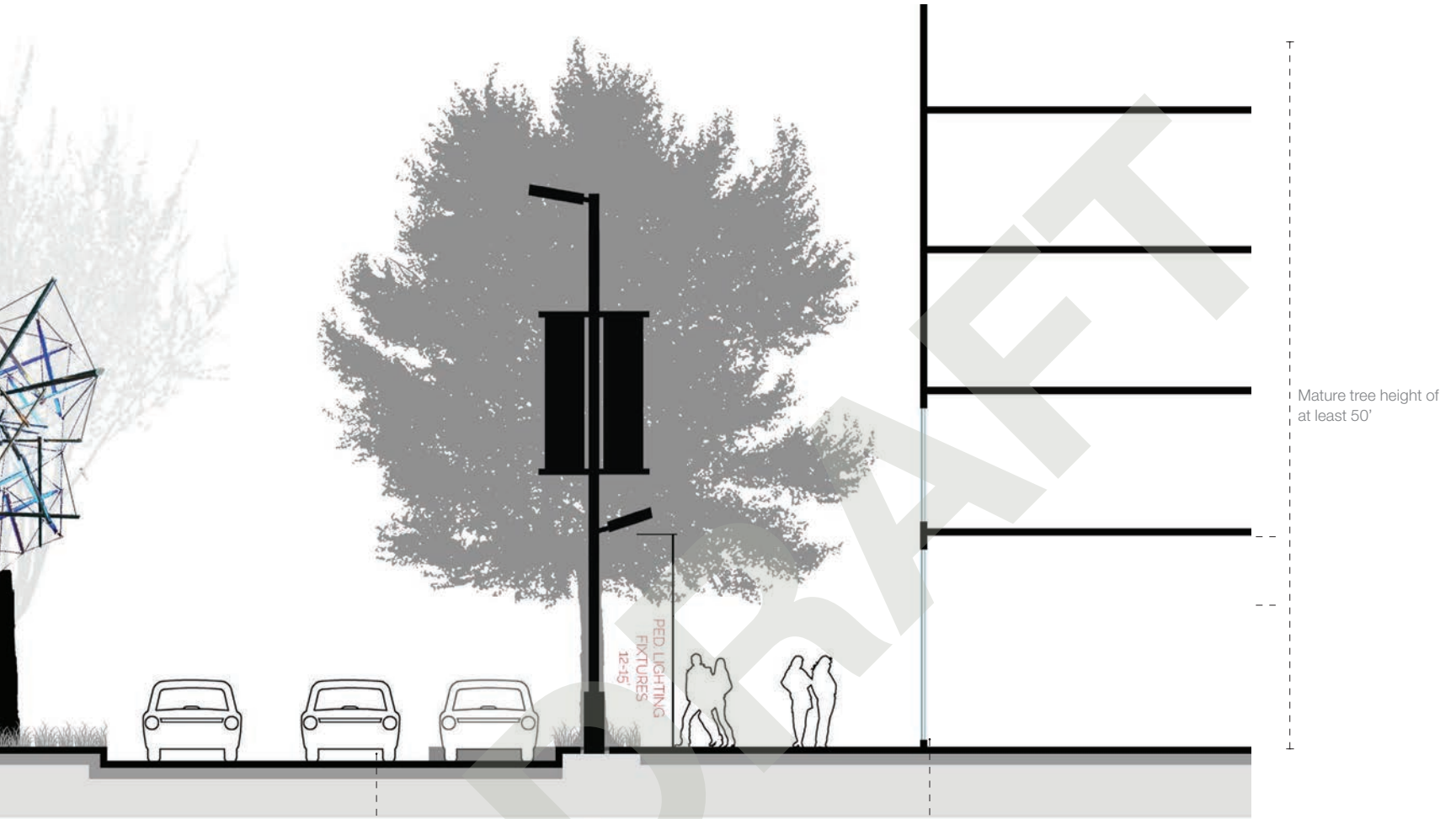
Paving Materials at Crosswalks

- a) Crosswalks and intersections should be clearly distinguished from the roadways and indicate the possible presence of pedestrians to approaching vehicles. Materials should be durable, long-lasting, and focused on pedestrian safety.

Signalization

- a) Advocate for pedestrian-activation, countdown, and pedestrian-prioritization signalization at all intersections.
- b) Coordinate traffic-signal selection with the Downtown Dallas 360 study to ensure coherent transportation scheme for all of Downtown Dallas.





MATURE CANOPY 40' MIN

TWO VEHICLE TRAVEL LANES
10'-11.5'

Lanes should be no more than 11.5' (except Flora street) to reduce travel speed and create a safer environment for non-motorized travel. Advocate for reduced, posted vehicle-travel speeds (25 mph) whenever possible

ON-STREET PARKING
8'-9'

PLANTING
4'-6'

PEDESTRIAN ZONE 6'-20'

Orient primary building entry points to the public realm in order to provide clear access and communicate the importance of public realm

Mature tree height of at least 50'

PEARL STREET: “AVENUE TO THE ARTS”

Vision

The Avenue to the Arts will reinterpret Pearl Street as a unifying connection across multiple neighborhoods. With a unique character consistent along its entire length, it will signify to users that the street serves an exceptional purpose in uniting Downtown via the arts.

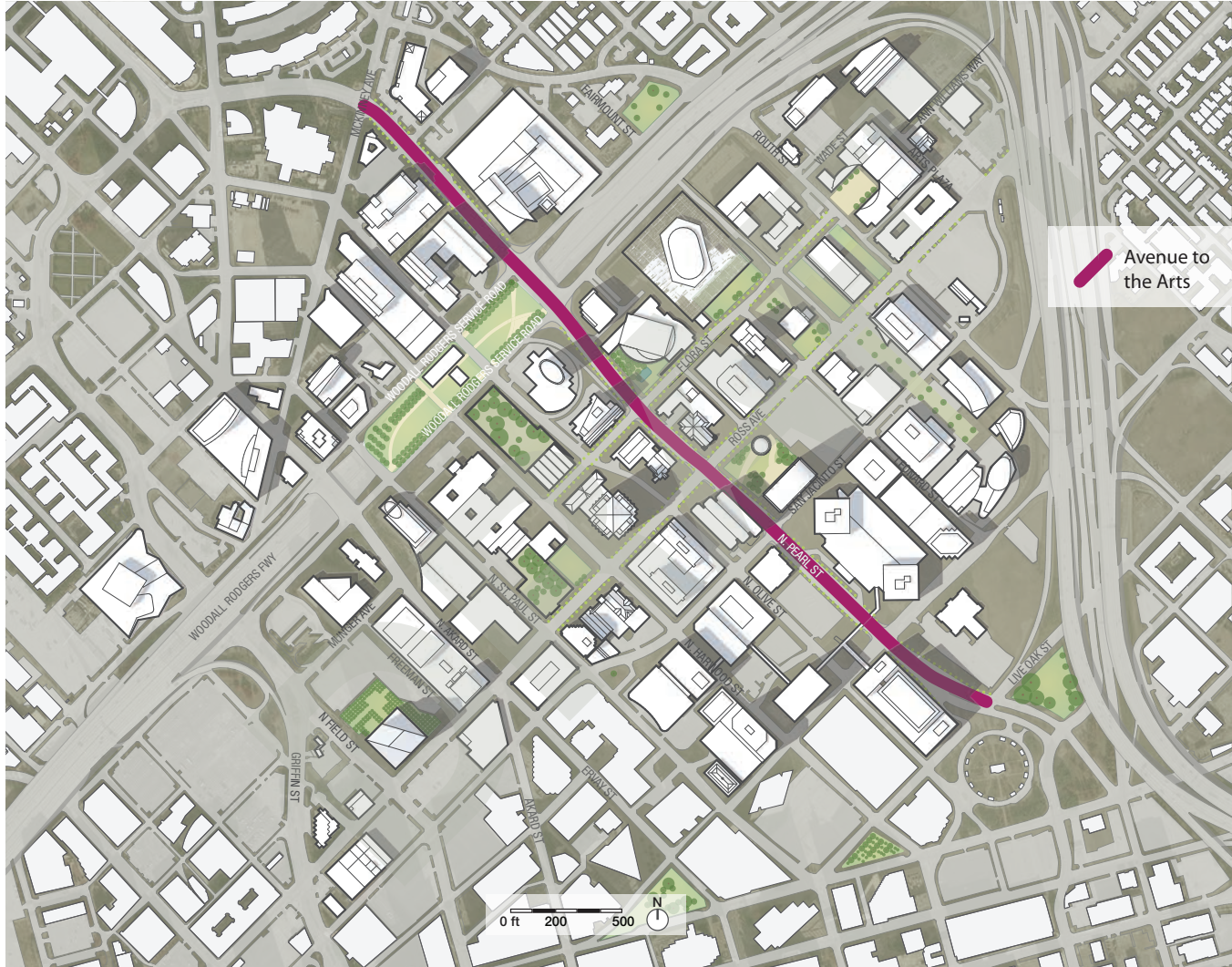
Pearl Street, given its role as an important connection across many Downtown neighborhoods, will share some of the same characteristics as District Connector streets.

Goals for Pearl Street

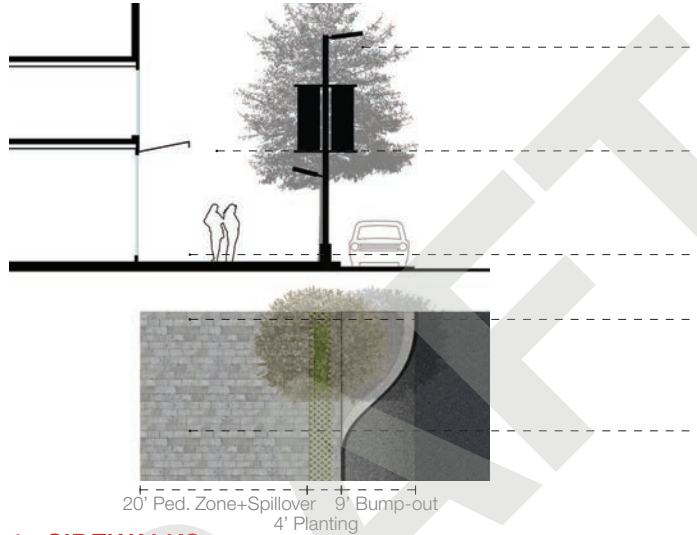
- 1) Wide, accessible sidewalks that balance pedestrian safety, environmental health, and support a mix of commercial and institutional uses.
- 2) Development controls that guarantee visual and physical access to active uses.
- 3) Public Art that engages and entices visitors to the Dallas Arts District.
- 4) Traffic calming that reduces the risk of harm at street crossings and improves pedestrian comfort.

Opposite page

Map showing extent of N. Pearl St. as the Avenue to the Arts, where it is most closely related to the Arts District.



Specific Design Guidelines



Street trees should closely resemble and complement those of Flora street in order to establish a consistent identity across the two main axes of the Dallas Art District.

Pedestrian zones should be wide and accessible, and balance pedestrian safety and environmental health while supporting a mix of commercial and institutional uses.

The public realm should accommodate all street furnishings and uses such as dining, retail spillover, and public art.

Paving materials should be continuous over the entire length of Pearl Street within the District.

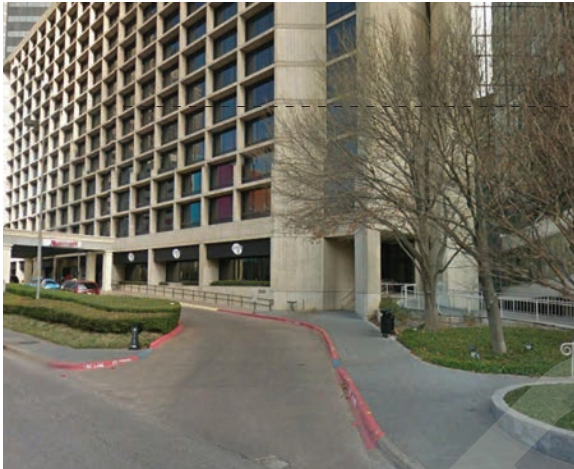
Exposed-aggregate concrete paving, broom-finished concrete paving, or concrete unit pavers with ADA-compliant surfaces are recommended for pedestrian zones along Pearl Street.

1. SIDEWALKS

- All District-wide design guidelines for Sidewalks apply to Pearl Street.
- Street trees should closely resemble and complement those of Flora Street in order to establish a consistent identity across the two main axes of the Dallas Arts District and emphasize Pearl Street's role as the Avenue to the Arts.
- Pedestrian zones should be wide and accessible, and balance pedestrian safety and environmental health while supporting a mix of commercial and institutional uses.
- Paving materials should be continuous over the entire length of Pearl Street within the District.
- Materials used should closely resemble and complement those of Flora Street to establish a consistent identity across the two main axes of the Dallas Arts District and emphasize Pearl Street's role as the Avenue to the Arts.
- Exposed aggregate concrete paving, broom finished concrete paving, or concrete unit pavers with ADA compliant surfaces are recommended for pedestrian zones along Pearl Street.

Top
Street elements

- Provide for sufficient stormwater infiltration and match or complement paving material on both Pearl and Flora Streets.
- The public realm should accommodate all street furnishings and uses such as dining, retail spillover, and public art.



Blank facades of horizontal lengths more than 30 ft and vertical heights more than 5 ft like that shown to the right are strongly discouraged



Materials used should closely resemble and complement those of Flora Street in order to establish a consistent identity across the two main axes of the Dallas Arts District and emphasize Pearl Street's role as the Avenue to the Arts

Top
Undesirable
street elements

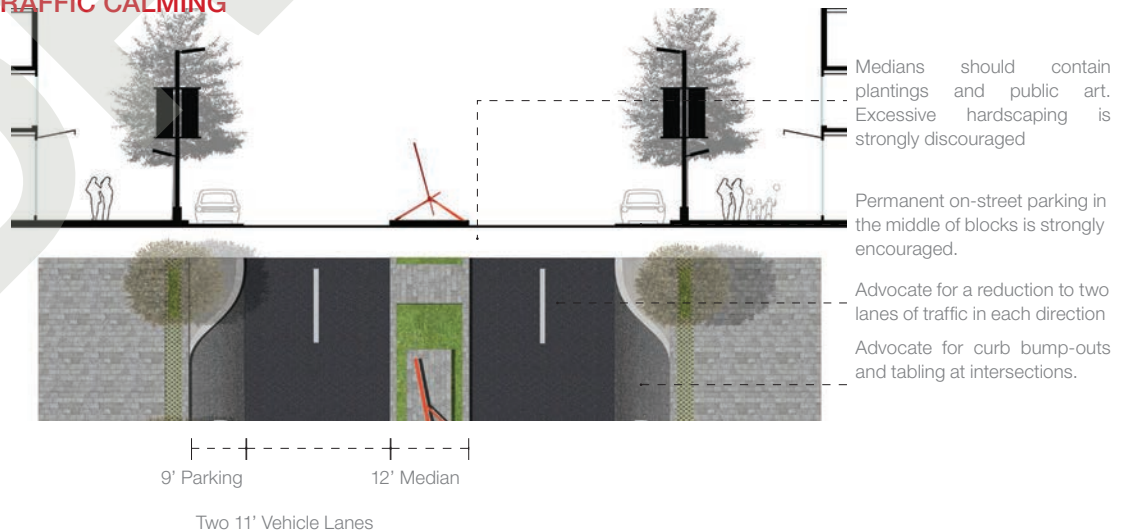
2. DEVELOPMENT CONTROLS

- All District-wide design guidelines for Development Controls apply to Pearl Street.
- The Plan recommends a maximum of 55 dBA at boundary line for continuous noise, 65 dBA for impact noise, or building noise not above existing ambient volumes. This building noise maximum does not apply to programming and events.
- Blank façades of horizontal lengths more than 30 ft and vertical heights more than 5 ft are strongly discouraged.
- Visible vehicle turnarounds are strongly discouraged.

3. PUBLIC ART

- All District-wide design guidelines for Public Art apply to Pearl Street.
- Public art features that will engage and entice visitors to the Dallas Arts District are strongly encouraged.
- Special considerations should be given to art installations along Pearl Street as it is considered "Avenue to the Arts."

4. TRAFFIC CALMING



Right
Street elements

- All District-wide design guidelines for Traffic Calming apply to Pearl Street.
- Permanent on-street parking in the middle of blocks is strongly encouraged.
- Advocate for the provision of a pedestrian refuge at crossing points to support pedestrian safety.
- Medians should contain plantings and public art. Excessive hardscaping is strongly discouraged.
- Advocate for curb bump-outs and tabling at intersections.
- Continue concrete block pavers edged by concrete band (flush curb) in roadway to mitigate vehicle speeds and communicate unique character while providing access to all users and ensuring durability.
- Maintain existing District-specific traffic signal and signage elements used on Flora Street.
- Add new signals per city standards.

DRAFT

FLORA STREET/ANN WILLIAMS WAY: MIXED-USE CULTURAL CORRIDORS

Vision

Flora Street/Ann Williams Way will remain the heart of the Dallas Arts District, embodying flexibility, quality, and sustainability. With a low-scale landscaped promenade and a unique, arts-focused character, the street will convey a sense of welcoming urbanism with a strong mix of retail and cultural uses. The arts will extend out of the performance halls and galleries to engage the public within a distinctive, urban atmosphere.

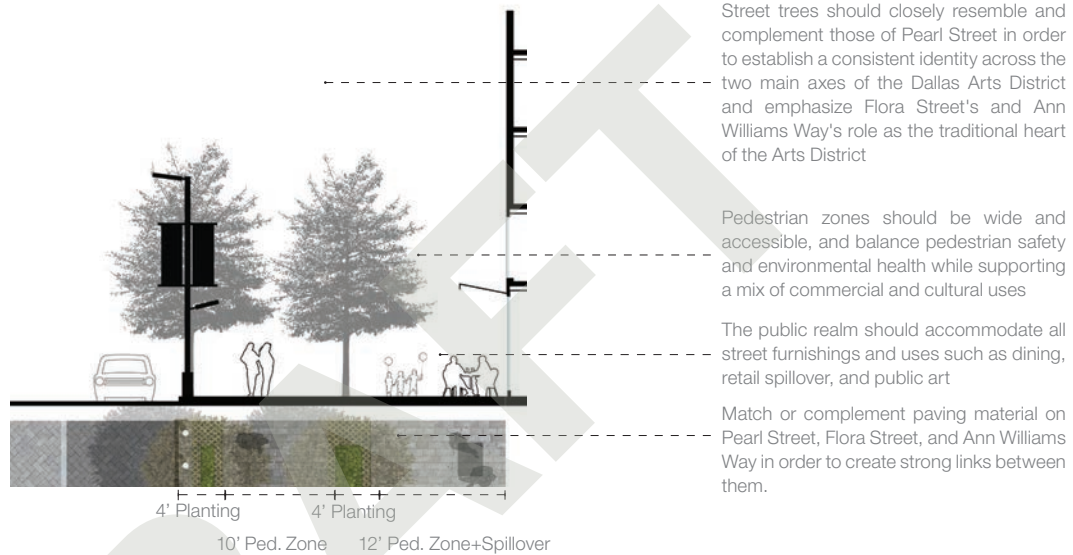
Goals for Flora Street and Ann Williams Way

- 1) Wide, accessible sidewalks that balance pedestrian safety, environmental health, and support a mix of residential, commercial, and institutional uses.
- 2) Traffic calming that reduces the risk of harm at street crossings and improves pedestrian comfort.
- 3) Development controls that encourage access to a lush environment and a wide range of active uses in a unique, low-scale environment.
- 4) Public Art that engages and entices visitors to the Dallas Arts District.
- 5) Although they are Pedestrian-Priority streets, Flora Street and Ann Williams Way should have enhancements that emphasize the physical and psychological connections between the two.

Opposite page
Map showing extent of Flora St., as a mixed-use cultural corridor.



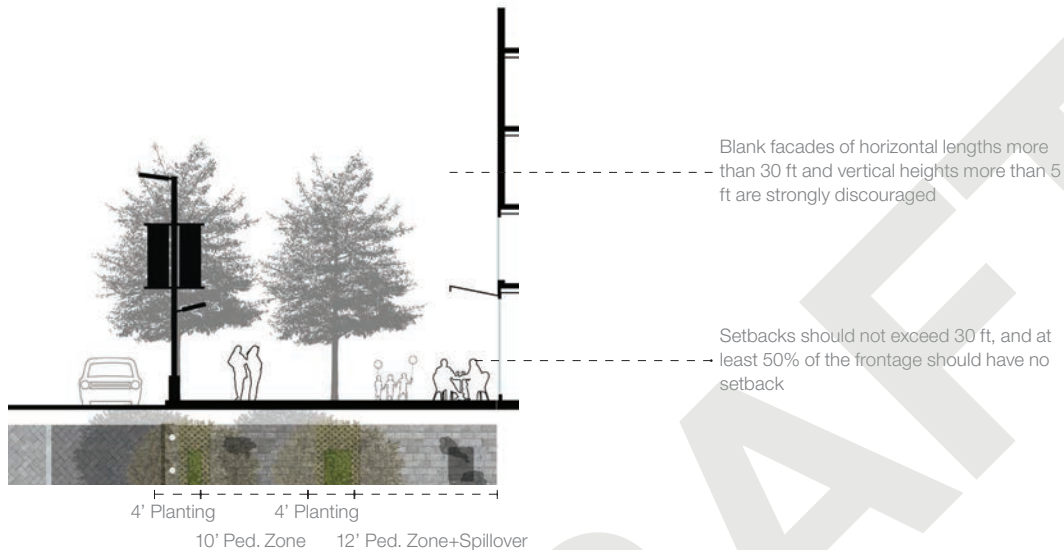
Specific Design Guidelines



Sidewalks

- All District-wide design guidelines for Sidewalks apply to Flora Street and Ann Williams Way.
- Street trees should closely resemble and complement those of Pearl Street in order to establish a consistent identity across the two main axes of the Dallas Arts District and emphasize Flora Street's and Ann Williams Way's role as the traditional heart of the Arts District.
- Pedestrian zones should be wide and accessible, and balance pedestrian safety and environmental health while supporting a mix of commercial and cultural uses.
- Paving materials should closely resemble and complement those of Pearl Street in order to establish a consistent identity across the two main axes of the Dallas Arts District and emphasize Flora Street's and Ann Williams Way's role as the traditional heart of the Arts District.

Top
Street elements



- Material should be smooth, large, and continuous along the entire length of Flora Street and Ann Williams Way. Exposed-aggregate concrete paving, broom-finished concrete paving or concrete unit pavers with ADA-compliant surfaces are recommended for pedestrian zones along Flora Street and Ann Williams Way.
- Provide for sufficient stormwater infiltration and match or complement paving material on both Pearl and Flora Streets.
- The public realm should accommodate all street furnishings and uses such as dining, retail spillover, and public art, as licensed.

Development Controls

- All District-wide design guidelines for Development Controls apply to Flora Street and Ann Williams Way.
- Setbacks should not exceed 30 ft.
- The Plan recommends a maximum of 55 dBA at Boundary line for continuous noise, 65 dBA for impact noise, or building noise not above existing ambient volumes.

Top
Street elements

- Blank façades of horizontal lengths more than 30 ft and vertical heights more than 5 ft are strongly discouraged.
- Visible vehicle turnarounds are strongly discouraged.

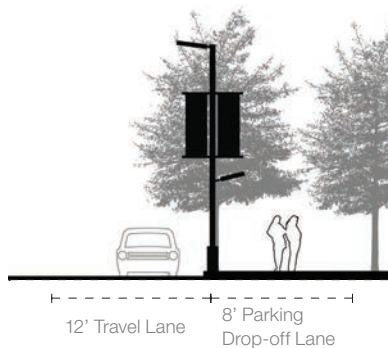
Public Art

- All District-wide design guidelines for Public Art apply to Flora Street and Ann Williams Way.
- Public art features that will engage and entice visitors to the Dallas Arts District are strongly encouraged.
- Public art along Flora Street and Ann Williams Way should support its role as part of the traditional heart of the Arts District.

Traffic Calming

- All District-wide design guidelines for Traffic Calming apply to Flora Street and Ann Williams Way.
- Continue use of pavers in roadway to mitigate vehicle speeds and communicate unique character.
- Continue use of bollards and flush curbs to allow flexible use of street and support the character of the neighborhood.

Top
Street elements



Continue concrete block pavers edged by concrete band (flush curb) in roadway to mitigate vehicle speeds and communicate unique character while providing access to all users and ensuring durability

Continue use of bollards and flush curbs to allow flexible use of street and support district character

Continue use of pavers in roadway to mitigate vehicle speeds and communicate unique character

- Continue concrete block pavers edged by concrete band (flush curb) in roadway to mitigate vehicle speeds and communicate unique character while providing access to all users and ensuring durability.

Additional Notes

- Though Ann Williams Way is considered a Pedestrian Priority street, effort should be made to align its character more closely with Flora Street. Ann Williams Way is an important thoroughfare within the Arts District, and though dimensional concerns limit some possible transformations, all proposed changes to that street should communicate its role as an integral part of the Arts District.

DRAFT

ROSS AVENUE: MIXED-USE COMMERCIAL CORRIDOR

Vision

The active commercial corridor contains a very high-density and extreme mix of uses. The high volumes of foot, transit, and auto traffic make it a hub of activity on the ground floor and encourage flexibility and outdoor amenities.

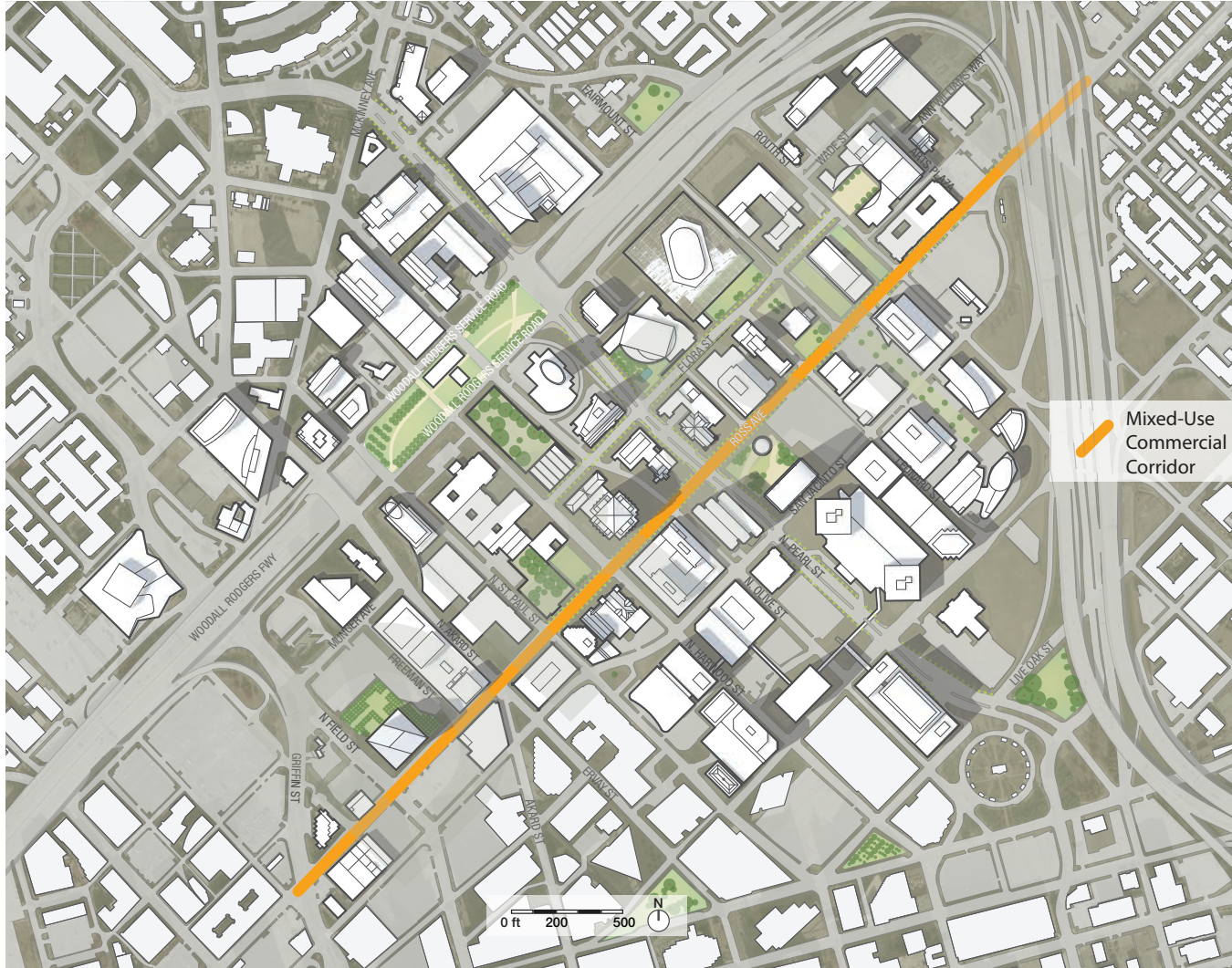
Ross Avenue, given its role as an important connection across many Downtown neighborhoods, will share some of the same characteristics as District Connector streets.

Goals for Ross Avenue

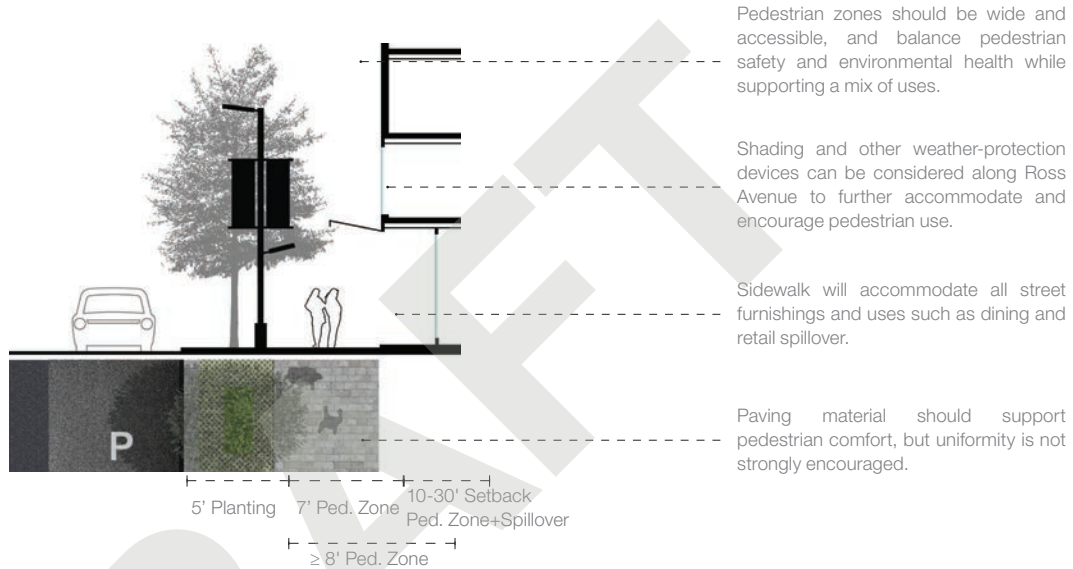
- 1) Wide, accessible sidewalks that balance pedestrian safety, environmental health, and a mix of uses.
- 2) Development controls that encourage access to a wide range of active uses and limit the impacts of building operations.
- 3) Traffic calming that reduce risks of harm and supports multi-modal transportation.

Opposite

Map showing extent of Ross Ave as it is most closely related to the Arts District.



Specific Design Guidelines



1. SIDEWALKS

- All District-wide design guidelines for Sidewalks apply to Ross Avenue.
- Pedestrian zones should be wide and accessible, and balance pedestrian safety and environmental health while supporting a mix of uses.
- Paving materials should support pedestrian comfort, but uniformity is not strongly encouraged.
- Broom-finished concrete paving or concrete-unit pavers with ADA-compliant surfaces are recommended in order to reduce maintenance needs and provide a safe, comfortable surface for large volumes of pedestrians and flexibility of use.
- Sidewalks will accommodate all street furnishings and uses such as dining and retail spillover.
- Shading and other weather-protection devices can be considered along Ross Avenue

[Top](#)
Street elements



Blank facades of horizontal lengths more than 30 ft and vertical heights more than 5 ft, like that shown to the right, are strongly discouraged.

Unlike the wide setback shown, setbacks should not exceed 30 ft and should consider the appropriate distance needed to accommodate desired pedestrian and ground floor uses.

Curb cuts like the one shown should be avoided or removed to reduce negative impacts of a discontinuous pedestrian environment punctuated by vehicle activities.

to further accommodate and encourage pedestrian use.

2. DEVELOPMENT CONTROLS

- All District-wide design guidelines for Development Controls apply to Ross Avenue.
- Setbacks should not exceed 30 ft, and should consider the appropriate distance needed to accommodate desired pedestrian and ground floor uses.
- The Plan recommends a maximum of 55 dBA at Boundary line for continuous noise, 65 dBA for impact noise, or building noise not above existing ambient volumes.
- Blank façades of horizontal lengths more than 30 ft and vertical heights more than 5 ft are strongly discouraged.
- Visible vehicle turnarounds are strongly discouraged.

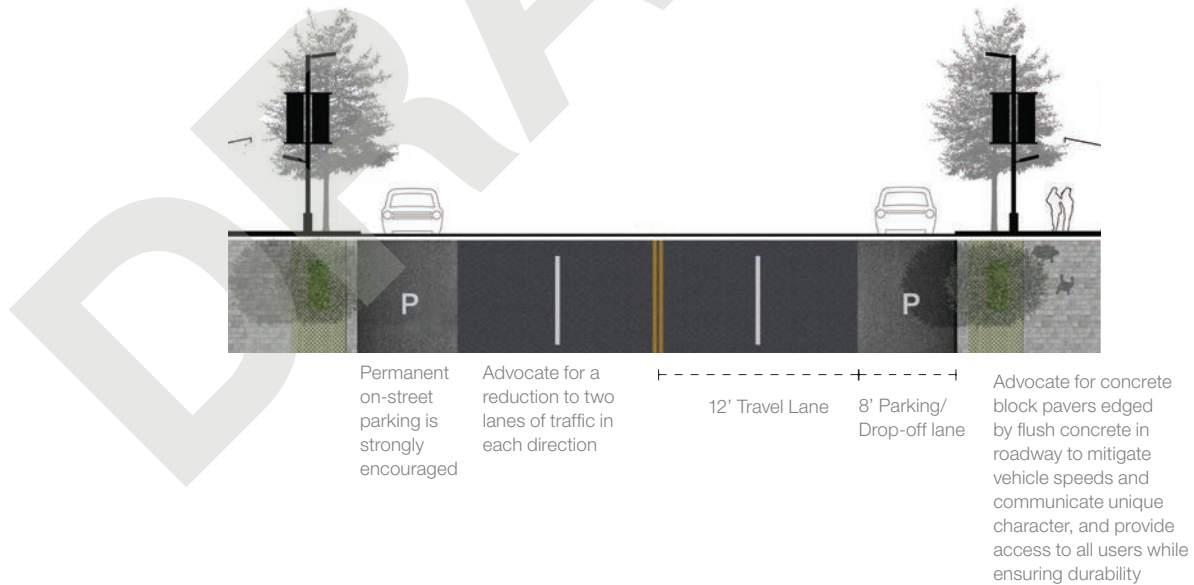
Top
Undesirable street
elements

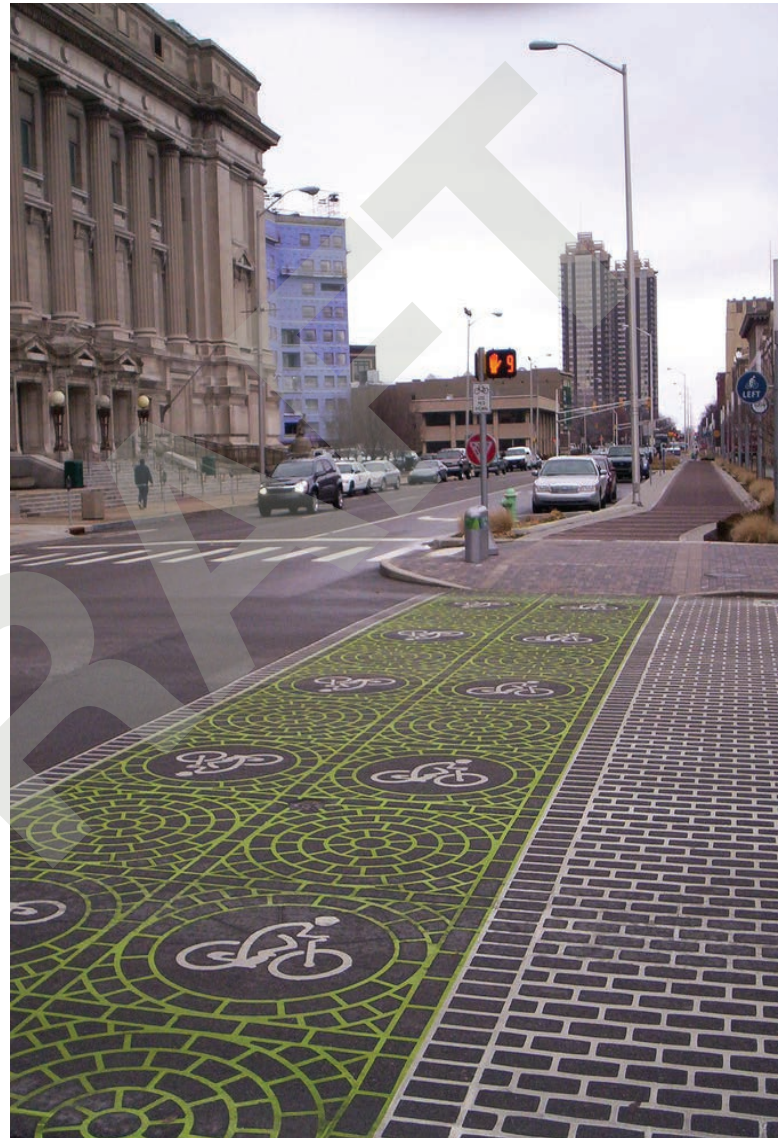
3. PUBLIC ART

- All District-wide design guidelines for Public Art apply to Ross Avenue.

4. TRAFFIC CALMING

- All District-wide design guidelines for Traffic Calming apply to Ross Avenue.
- Permanent on-street parking to provide quick access to parking, and buffer pedestrians is strongly encouraged.
- Advocate for a reduction to two lanes of traffic in each direction.
- Advocate for concrete-block pavers edged by flush concrete in roadway to mitigate vehicle speeds, communicate unique character, and provide access to all users while ensuring durability.





Above and Right

Dedicated, protected bike lanes, tree shading, diverse building facades, bollard fencing, and other street design techniques enable safe, multi-modal transportation in Indianapolis, Indiana.
Photos courtesy Flickr user Stellascorcher

Opposite

Street section showing elements of Ross Ave as a mixed-use commercial corridor.

PEDESTRIAN PRIORITY STREETS

(N. Harwood St., Leonard St., Crockett St., Wade St., N. Olive St., Woodall Rodgers Frontage Roads, McKinney, N. St. Paul St., N. Akard St., Flora St., and Ann Williams Way)

Vision

Pedestrian Priority Streets will provide a more serene experience for those traveling on foot by significantly calming vehicles and enhancing pedestrian and bicycle accommodations and green amenities. Some Pedestrian Priority streets will share some characteristics with District Connectors. However, these streets are intended to more specifically focus on changes to the pedestrian environment that improve comfort more than District Connectors.

Goals for Pedestrian Priority Streets

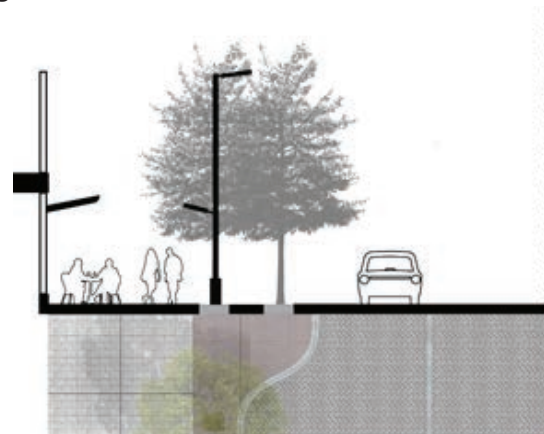
- 1) Wide, accessible sidewalks that balance pedestrian safety, environmental health, and a mix of uses.
- 2) Development controls that encourage access to active uses, limit the impacts of building operations, and support pedestrian and cyclist comfort.
- 3) Traffic calming that reduces the risk of harm and supports multi-modal transportation.

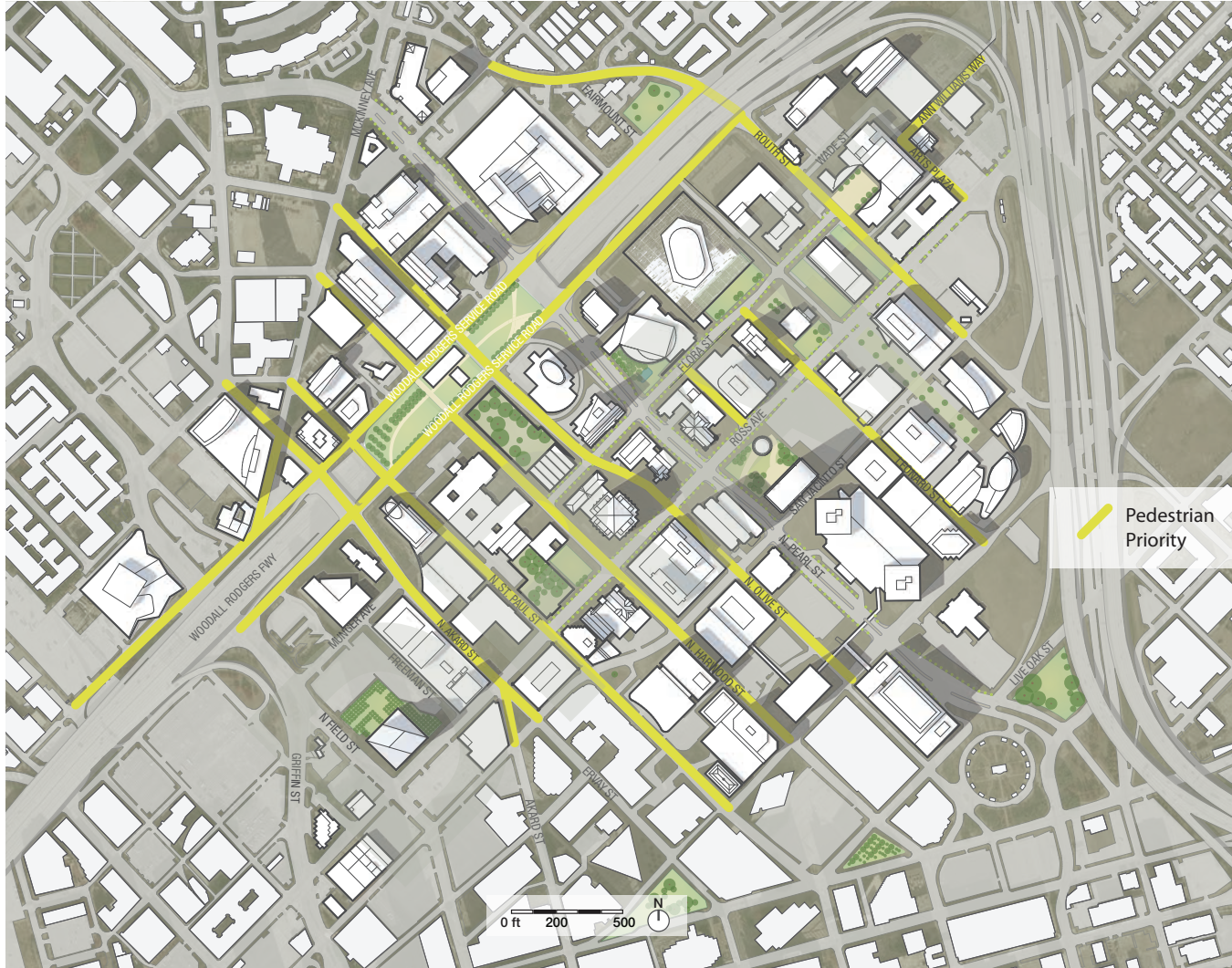
Right

Street section showing elements of Pedestrian Priority streets.

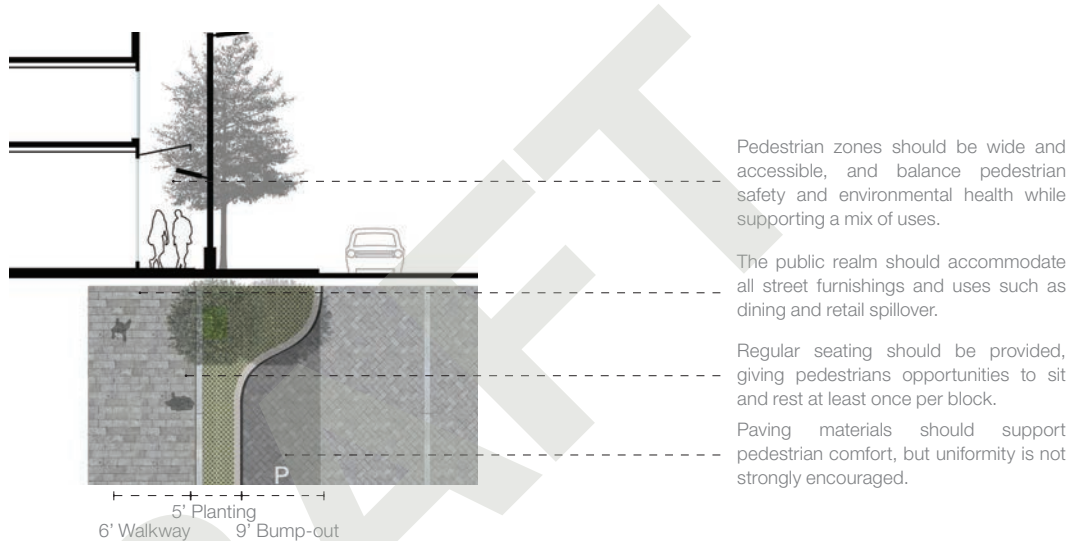
Opposite

Map showing extent of Pedestrian Priority streets.





Specific Design Guidelines



1. SIDEWALKS

- All District-wide design guidelines for Sidewalks apply to Pedestrian Priority Streets.
- Pedestrian zones should be wide and accessible, and balance pedestrian safety and environmental health while supporting a mix of uses.
- Paving materials should support pedestrian comfort, but uniformity is not strongly encouraged.
- Exposed-aggregate concrete paving or concrete-unit pavers with ADA-compliant surfaces are recommended to reduce maintenance needs and provide a safe, comfortable surface for pedestrians and flexibility of use.
- Regular seating should be provided, giving pedestrians opportunities to sit and rest at least once per block.

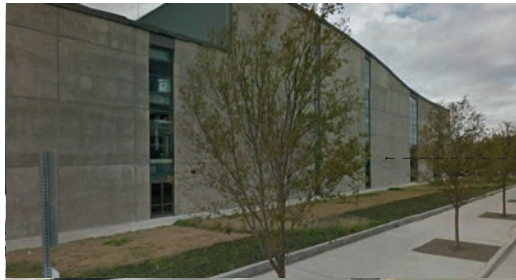
Top
Street elements



Avoid such features as parking and garage entrances on these streets when possible in order to maintain continuous pedestrian experience

The size of vehicle access points should be limited

Loading docks are allowed where necessary but not encouraged, in order to maintain a continuous pedestrian experience



Blank facades of horizontal lengths more than 30 ft and vertical heights more than 5 ft are strongly discouraged

- The public realm should accommodate all street furnishings and uses such as dining and retail spillover.

2. DEVELOPMENT CONTROLS

- All District-wide design guidelines for Development Controls apply to Pedestrian Priority Streets.
- Loading docks should only be placed when and where necessary in order to maintain a safe, continuous pedestrian experience. Avoid such features as parking and garage entrances on these streets when possible in order to maintain continuous pedestrian experience.
- The size of vehicle-access points should be limited to minimize disruptions to a well-defined street edge.

Top
Undesirable street
elements

- Blank façades of horizontal lengths more than 30 ft and vertical heights more than 5 ft are strongly discouraged.

3. PUBLIC ART

- All District-wide design guidelines for Public Art apply to Pedestrian Priority Streets.
- Public art along Ann Williams Way should support its role as part of the traditional heart of the Arts District.

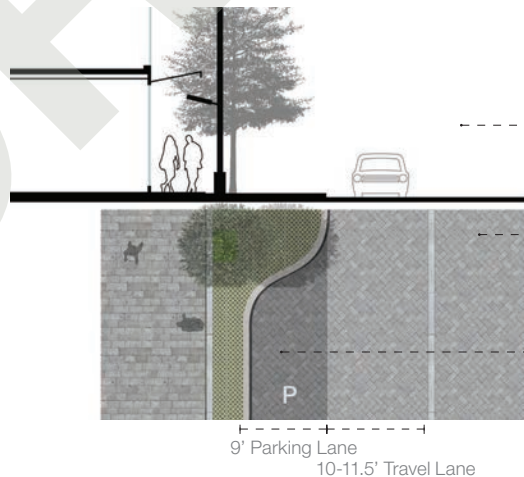
4. TRAFFIC CALMING

- All District-wide design guidelines for Traffic Calming apply to Pedestrian Priority Streets.
- Limit lane widths wherever possible. A maximum of 11' for multilane roads should be pursued to reduce highway like environments.
- Streets may also be brick or pavers, in order to slow traffic and encourage shared use.
- Advocate for curb bump-outs at intersections to reduce pedestrian-crossing distances, reduce roadway width, and provide permanent on-street parking.
- Advocate for concrete paver edged by concrete band (flush curb) to mitigate vehicle speeds, provide access for all users, and ensure durability.

Right
Traffic Calming features of Pedestrian Priority streets.

Opposite
Atlanta's Broad Street provides a wide pedestrian walkway including cafe and restaurant seating, tree shading, and on-street parking.

Photo courtesy of the National Association of City Transportation Officials (NACTO)



Limit lane widths wherever possible. A maximum of 11' ft for multi-lane roads should be pursued to reduce highway-like environments.

Streets may also be brick or pavers, in order to slow traffic and encourage shared use.

Advocate for curb bump-outs at intersections to reduce pedestrian crossing distances, reduce roadway width and provide permanent on-street parking.

Additional Notes

- Though Ann Williams Way is considered a Pedestrian Priority street, efforts should be made to align its character with Flora Street. Ann Williams Way is an important thoroughfare within the Arts District, and although dimensional concerns limit some possible transformations, all proposed changes to that street should communicate its role as an integral part of the Arts District.



DISTRICT CONNECTORS

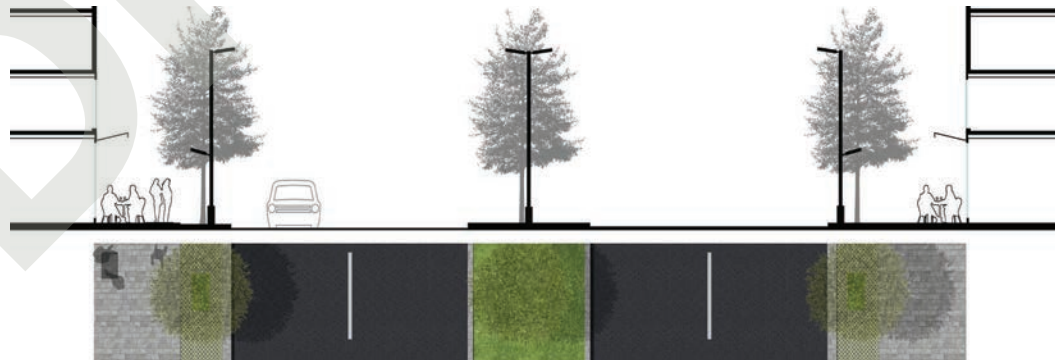
(N. Field St., N. Griffin St., Woodall Rodgers Frontage Road)

Vision

District Connector streets will facilitate high volumes of multimodal traffic and improve infrastructure for alternative transportation. Serving as a critical link across Downtown Dallas, it will facilitate pedestrians, bike, transit, and car travel between neighborhoods while ensuring safety is a priority.

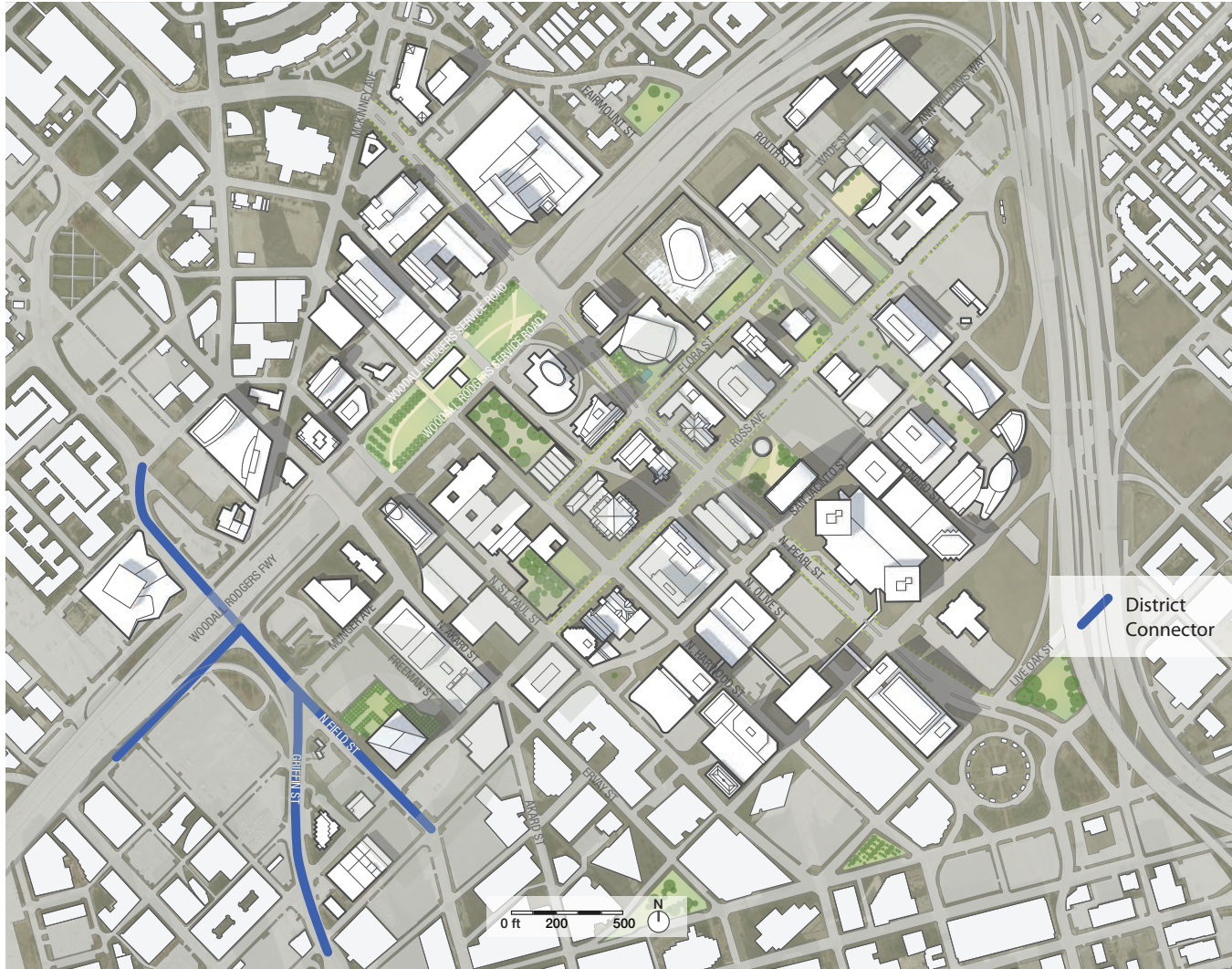
Goals for District Connectors

- 1) Wide, accessible sidewalk that balance pedestrian safety with city-wide and regional transportation needs.
- 2) Traffic calming that reduces the risk of harm and supports multi-modal transportation.
- 3) Development controls that support pedestrian comfort and links between neighborhoods.

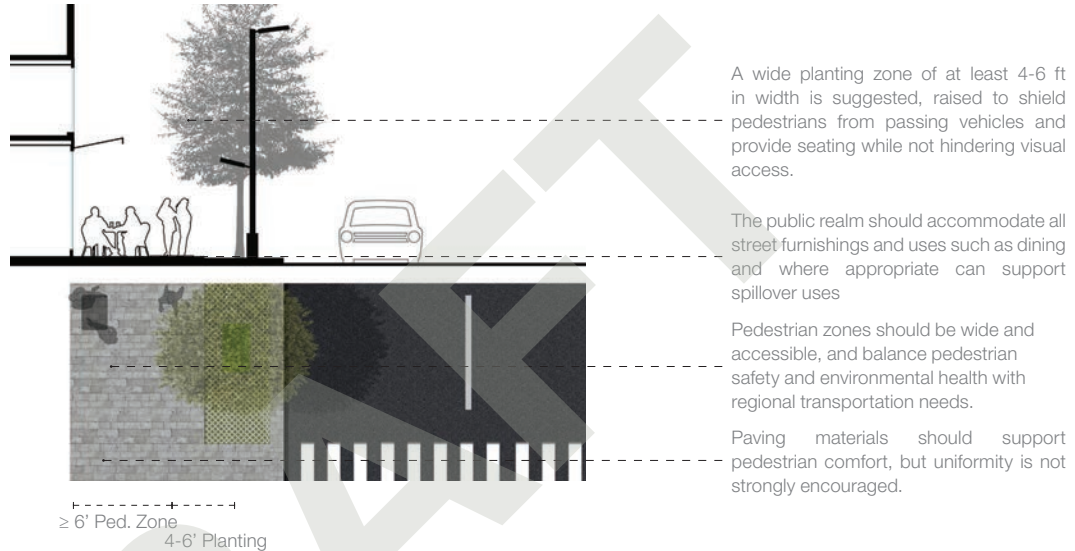


Right
Street section showing elements of District Connectors.

Opposite
Map showing extent of District Connectors.



Specific Design Guidelines



1. SIDEWALKS

- All District-wide design guidelines for Sidewalks apply to District Connectors.
- A wide planting zone of at least 4-6 ft in width is suggested, with periodic raised planters to shield pedestrians from passing vehicles and provide seating while not hindering visual access.
- Pedestrian zones should be wide and accessible, and balance pedestrian safety and environmental health with regional transportation needs.
- Paving materials should support pedestrian comfort, but uniformity is not strongly encouraged.
- The public realm should accommodate all street furnishings and uses such as dining and where appropriate can support spillover uses.

Above
Sidewalk design features
on District Connectors.



Blank facades of horizontal lengths more than 30 ft and vertical heights more than 5 ft are strongly discouraged.

Loading docks are allowed where necessary but not encouraged, in order to maintain a continuous pedestrian experience.

The size of vehicle-access points should be limited.

Avoid such features as parking and garage entrances on these streets when possible in order to maintain continuous pedestrian experience.

2. DEVELOPMENT CONTROLS

- All District-wide design guidelines for Development Controls apply to District Connectors.
- Loading docks should only be placed when and where necessary in order to maintain a safe, continuous pedestrian experience. Avoid such features as parking and garage entrances on these streets when possible in order to maintain continuous pedestrian experience.
- The size of vehicle-access points should be limited.
- The Plan recommends a maximum of 55 dBA at Boundary line for continuous noise, 65 dBA for impact noise, or building noise not above existing ambient volumes.

3. PUBLIC ART

- All District-wide design guidelines for Public Art apply to District Connectors.

4. TRAFFIC CALMING

- All District-wide design guidelines for Traffic Calming apply to District Connectors.

Above

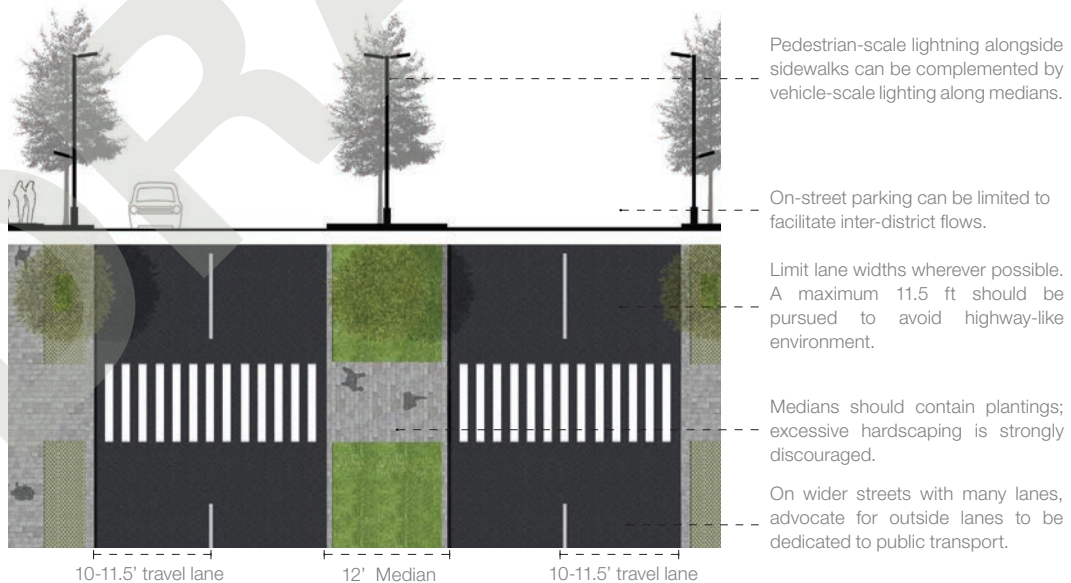
Examples of the kinds of development patterns that are not encouraged along District Connectors

- Limit lane widths wherever possible. A maximum 11.5 ft should be pursued to avoid highway-like environment.
- On-street parking can be limited to facilitate inter-district flows.
- Pedestrian-scale lighting alongside sidewalks can be complemented by vehicle-scale lighting along medians.
- On wider streets with many lanes, advocate for outside lanes to be dedicated to public transit.
- Advocate for the provision of a pedestrian refuge at crossing points to support pedestrian safety.
- Medians should contain plantings; excessive hardscaping is strongly discouraged.
- Advocate for concrete paver edged by concrete band (flush curb) to mitigate vehicle speeds, provide access for all users, and ensure durability.

Right
Traffic Calming features on District Connectors.

Opposite
A pedestrian refuge island and signage calm traffic and increase pedestrian safety in Atlanta, GA.

Photo courtesy of Flickr user NACTO





SERVICE-ORIENTED STREETS

(San Jacinto St., Munger Ave, Woodall Rodgers Frontage Rds, Jack Evans St., Caroline St., Freeman St., Federal St., Crockett St., Leonard St.)

Vision

Service-Oriented streets will accommodate most of the operational needs of the District. With loading docks, garage entrances, and narrower widths, they will limit vehicular throughput and keep the necessities of building operations away from other pedestrian-friendly streets.

Goals for Service-Oriented Streets

- 1) Basic sidewalks that balance pedestrian safety with building operations.
- 2) Development controls that minimize the negative impacts of operations on street life and design quality.
- 3) Traffic calming that reduces risk of harm and supports building operations.

Right

Street section showing elements of Service-Oriented streets.

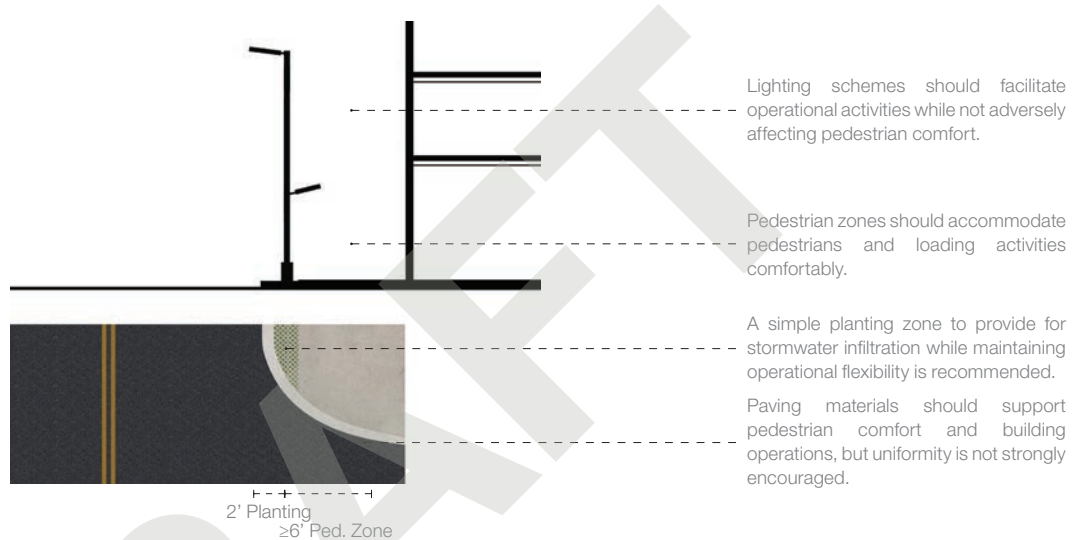
Opposite

Map showing extent of Service-Oriented streets.





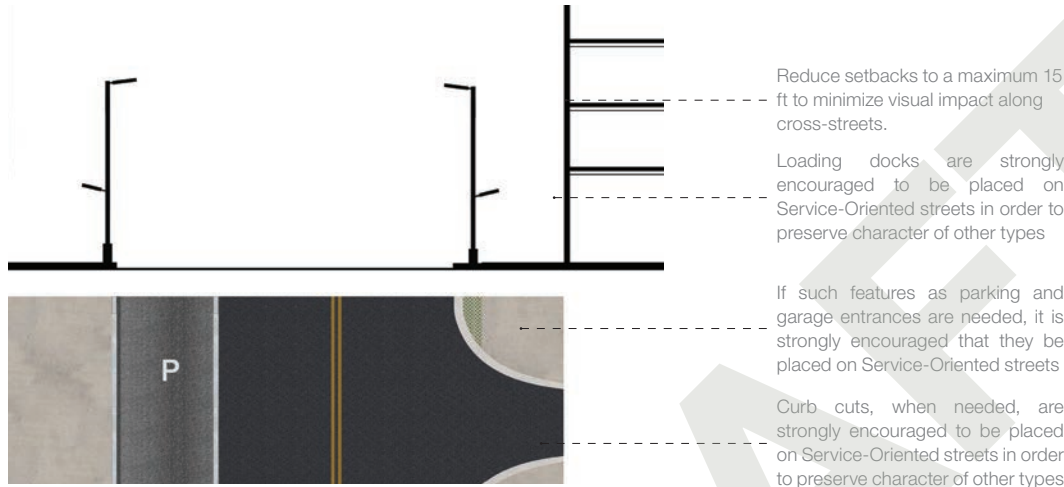
SPECIFIC DESIGN GUIDELINES



Sidewalks

- All District-wide design guidelines for Sidewalks apply to Service-Oriented Streets except as noted.
- A simple planting zone to provide for stormwater infiltration while maintaining operational flexibility is recommended.
- Consider trees as needed. Proposed trees should not disrupt building operations.
- Pedestrian zones should accommodate pedestrians and loading activities comfortably.
- Paving materials should support pedestrian comfort and building operations, but uniformity is not strongly encouraged.
- Lighting schemes should facilitate operational activities while not adversely affecting pedestrian comfort.

Top
Desirable
street elements



Development Controls

- All District-wide design guidelines for Development Controls apply to Service-Oriented streets.
- Reduce setbacks to a maximum 15 ft to minimize visual impact along cross-streets.
- Curb cuts, when needed, are strongly encouraged to be placed on Service-Oriented streets in order to preserve character of other types.
- Loading docks are strongly encouraged to be placed on Service-Oriented streets in order to preserve character of other types.
- If such features as parking and garage entrances are needed, it is strongly encouraged that they be placed on Service-Oriented streets.
- It is strongly recommended that utilities be placed along Service-Oriented streets in order to preserve character of other types.

Top
Desirable
street elements

- If such utility systems must be placed within sight, it is recommended that they be placed on Service-Oriented streets in order to preserve character of other types.

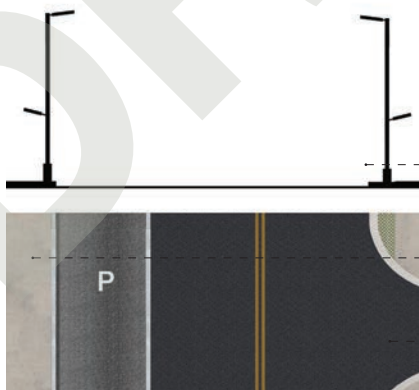
Public Art

- All District-wide design guidelines for Public Art apply to Service-Oriented streets.

Traffic Calming

- All District-wide design guidelines for Traffic Calming apply to Service-Oriented streets.
- On-street parking to allow for loading and quick drop-offs.
- Service-oriented streets may be one-way to reduce overall widths and minimize impacts on perpendicular streets.
- Lane widths should be limited to discourage fast travel speeds and reduce disruptions to cross-streets.
- Advocate for concrete paver edged by concrete band (flush curb) to mitigate vehicle speeds, provide access for all users, and ensure durability.

Right
Desirable
street elements



Service-oriented streets may be one-way to reduce overall widths and minimize impact on perpendicular streets.

On-street parking to allow for loading and quick drop-offs.

Lane widths should be limited to discourage fast travel speeds and reduce disruptions to cross-streets.



Right
Service-Oriented
Streets and alleyways
that are accessible to
pedestrians and also
provide space for retail
and commercial uses
*Photos courtesy Flickr
user NACTO*

APPENDIX

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APPENDIX I

BOND IMPLEMENTATION PLAN AND CONCEPTUAL REPRESENTATIONS

Overview

Through the District 14 local allocation for street projects, the Dallas Arts District received \$1.25 million in bond funds for pedestrian and streetscape improvements at Pearl, Flora, and Routh Streets as a match to over \$1 million in private funds.

Timeline

- Fall 2016:** Presented initial request for funds to City
- Jan 2017:** Revised request for bond funds to comply with City match requirement
- Jan 2017:** Bond election delayed from May to November 2017
- Spring 2017:** Participated in Citizen Bond Task Force process
- Summer 2017:** Advocated with Councilmember Kingston and City staff for local project funds
- Aug 2017:** Notified by City staff that DAD request included in D14 street allocation
- Oct 2017:** Endorsed "Perfect 10" citywide bond campaign
- Nov 2017:** Dallas voters overwhelmingly approved all 10 bond propositions, including Proposition A with DAD street funding
- Jun 2018:** Beginning of monthly meetings on Routh Street Gateway between Billingsley Co., Corgan, and Dallas Arts District

Funding Detail

Dallas bond package for Pearl/Flora intersection and Routh Street Gateway	\$1.250 million
Committed private matches	\$1.175 million
Available project funds	\$2.425 million

Routh Street Gateway:

- \$225,000 bond request;
- \$100,000 private match from Billingsley;
- \$75,000 private match from Downtown Dallas, Inc.; and
- Uptown Dallas previously committed \$75,000 match and it is currently pending Executive Committee approval.

Pearl/Flora St. intersection improvements:

- \$1,000,000 bond request;
- \$1,000,000 match from Atelier Flora Lofts.

Bond process

The City Manager has promised that all bond funds will be committed within 5 years. Working with the Bond Program Office and Councilmember Kingston, bond funds can be scheduled for sale and release as needed. A bond project - and its funds - may be divided into necessary segments and timelines.

BOND IMPLEMENTATION

PEARL ST.: AVENUE TO THE ARTS



ROUTH STREET GATEWAY

OPTION A1:

ENHANCED CONCRETE
PAVING

PIN MOUNTED LETTERS
ON EXISTING WOODALL
BRIDGE

20' WIDE ILLUMINATED
ARCHITECTURAL ELEMENT
AT SIDEWALK

PAINT ALONG FACE OF
BRIDGE

EXTEND STRUCTURE
ABOVE WOODALL

STRUCTURAL COLUMNS
WRAPPED & ILLUMINATED



APPENDIX II

STAKEHOLDER OUTREACH DOCUMENTATION

Meeting

Meeting Date

Meyerson Symphony Center + DCPH	2/8/2016
Infrastructure Committee	2/8/2016
Trammell Crow	2/8/2016
Dallas Museum of Art	2/8/2016
Booker T Washington HS	2/8/2016
Perot Museum	2/8/2016
Downtown Dallas, Inc./MIG/City Planning & Design	2/9/2016
First United Methodist Church of Dallas	2/9/2016
Dallas Symphony Orchestra	2/17/2016
Billingsley Company	2/29/2016
Greene Oglesby	2/29/2016
Matthews Southwest	2/29/2016
AT&T PAC	2/29/2016
Uptown Dallas, Inc.	2/29/2016
Nasher Sculpture Center	2/29/2016
Dallas Innovation Alliance	2/29/2016
Dallas Theatre Company, Dallas Arts District Foundation	3/1/2016
Hall Group	3/3/2016
Dallas Black Dance Theatre	7/18/16
CM Casey Thomas	8/22/2016
CM Erik Wilson	8/22/2016

COD Chief Planning Officer	8/22/2016
CM Monica Alonzo	8/23/2016
CM Jennifer Gates	8/23/2016
CM Lee Kleinman	8/23/2016
CM Ricky Callahan	9/6/2016
First Reading on the Bond	9/7/2016
CM Sandy Greyson	9/14/2016
Meadows Foundation, Lyda Hill Foundation, Catherine and Deedie Rose	9/19/2016
CM Scott Griggs	9/20/2016
Dallas Morning News	9/20/2016
CM Phillip Kingston	9/27/2016
COD Sustainable Development and Construction	9/27/2016
Dallas Parks and Recreation Dept.	9/27/2016
Mayor Rawlings	10/12/2016
Dallas Morning News Editorial Board	10/17/2016
TX Trees Foundation	10/17/2016
Phoenix Restaurant Group	10/17/2016
1900 Pearl St. - Lincoln Properties	10/17/2016
Park Cities Rotary Club	10/21/2016
Uptown Dallas Inc.	11/1/2016
Dallas Promenade	11/7/2016
City Council - Arts, Culture, Libraries Committee	11/7/2016
Uptown Dallas, Inc.	11/7/2016

STAKEHOLDER OUTREACH DOCUMENTATION

Klyde Warren Park	11/7/2016
Perot Museum	11/7/2016
St. Paul United Methodist Church	11/7/2016
Klyde Warren Park	11/14/2016
Oak Cliff Lions Club	1/11/2017
Leadership Arts Board Meeting	1/17/2017
Museum Tower	1/25/2017
DAD Stakeholder Meeting	2/27/2017
Spire Realty	3/29/2017
Klyde Warren Park	4/11/2017
One Arts Residences	4/11/2017
Billingsley Company	5/3/2017
CC Young Retirement Community	5/6/2017
FUMC Leaders + church leaders	5/9/2017
Downtown 360 Update	5/22/2017
BTWHSPVA Strategic Planning Meeting on plan	5/22/2017
Crow Museum of Asian Art, Executive Committee	6/1/2017
Visit Dallas	6/21/2017
Museum Tower	6/21/2017
Deep Ellum Foundation	Requested
Uptown Dallas, Inc.	Requested
Meadows Foundation	7/18/2017
Dallas Innovation Alliance	Week of 7/17/2017

Opposite

Dancers perform during the annual Dallas Arts District Block Party

One Arts Residents	8/15/2017
Spire Realty	8/31/2017
HALL Group	8/22/2017
Spire Realty	8/31/2017
Billingsley Company	9/15/2017
Spire, Trammell Crow Center (TCC), HALL Group, Atelier Flora Lofts	1/16/2018
Billingsley Company	1/23/2018
Planning Staff	2/23/2018
Fortis Group	5/11/2018
TCC, HALL Group, Atelier Flora Lofts, Billingsley Co., Fortis	5/18/2018
Spire Realty	6/6/2018
Dallas City Staff	6/8/2018
Billingsley Company	7/23/2018
Uptown District	7/23/2018
Deep Ellum Foundation	8/14/2018
First City of Dallas Community Meeting	11/12/2019
First City of Dallas Steering Committee Meeting	2/4/2020
Second City of Dallas Steering Committee Meeting	2/18/2020
Third City of Dallas Steering Committee Meeting	3/3/2020
Fourth City of Dallas Steering Committee Meeting	7/30/2020
Second City of Dallas Community Meeting	9/24/2020
City of Dallas Urban Design Advisory Committee	10/15/2020
City of Dallas Urban Design Advisory Committee	10/29/2020
Dallas CPC Briefing on PD 145, PD 708, CONNECT Master Plan	11/19/2020
Dallas CPC Briefing and Action PD 145, PD 708, CONNECT Master Plan	TBD

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